



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:
Anthony Lee Washington III
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HDR ENGINEERING, INC.
120 BRENTWOOD COMMONS WAY
SUITE 525
BRENTWOOD, TN 37027
ANTHONY L. WASHINGTON III, P.E. NO. 119749

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS, AND	
STANDARD TRAFFIC DESIGN DRAWINGS	1A
PROJECT COMMITMENTS.....	1B
ESTIMATED ROADWAY QUANTITIES.....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B, 2B1-2B4
GENERAL NOTES.....	2C
SPECIAL NOTES	2D
ENVIRONMENTAL NOTES.....	2E, 2E1
TABULATED QUANTITIES.....	2F, 2F1, 2F2
UTILITY NOTES AND UTILITY OWNERS.....	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL.....	4
BRIDGE CLOSURE DETOUR ROUTE.....	5, 5-1

YEAR	PROJECT NO.	SHEET NO.
2025	791240-S8-002	ROADWAY-SIGN 1

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET



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2025.06.23 17:33:13-05'00'
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SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN2
TITLE SHEET	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS, AND STANDARD TRAFFIC DESIGN DRAWINGS	1A
ESTIMATED ROADWAY QUANTITIES	2

YEAR	PROJECT NO.	SHEET NO.
2025	79I240-S8-002	ROADWAY-SIGN 2

REV. 06-23-25: ADDED SHEET

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET



THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN3
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS, AND	
STANDARD TRAFFIC DESIGN DRAWINGS	1A

YEAR	PROJECT NO.	SHEET NO.
2025	79I240-S8-002	ROADWAY-SIGN 3

REV. 06-30-25: ADDED SHEET

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET

Index Of Sheets
SEE SHEET NO. 1A

ADJACENT PROJECTS
PIN 132470.00 L.M. 12.71 TO L.M. 15.00 RESURFACING
PIN 134735.12 L.M. 0.00 TO L.M. 6.00 LET PROJECT - BI-PED SAFETY ACTIVITIES

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

SHELBY COUNTY

I-240
FROM: L.M. 0.00 (I-40)
TO: L.M. 6.00 (I-55)

BEGIN PROJECT NO. 79I240-S8-002 RESURFACE AND SAFETY
L.M. 0.00 (I-40)

RESURFACE AND SAFETY
MILL, 411D, 411E, GUARDRAIL AND PAVEMENT MARKINGS

STATE HIGHWAY NO. N/A F.A.H.S. NO. I-240

ILLINOIS CENTRAL RAILROAD COMPANY CROSSING #299616C
I-240 L.M. 4.54
ILLINOIS CENTRAL RAILROAD COMPANY (0394.810)

BRIDGE DECK REPAIR PROJECT NO. 79I240-M3-003
I-240 L.M. 5.34
I-240 L.M. 5.48

END PROJECT NO. 79I240-S8-002 RESURFACE AND SAFETY
L.M. 6.00 (I-55)

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT PROJECT MANAGER : LYNN EVANS, P.E., REG. 4

DESIGNED BY : HDR ENGINEERING, INC.

DESIGNER : ANTHONY L. WASHINGTON, III, P.E. CHECKED BY DAVID HORNE, P.E.

P.E. NO. 98043-4175-04

PIN NO. 129084.00



SCALE: 1" = 5280'

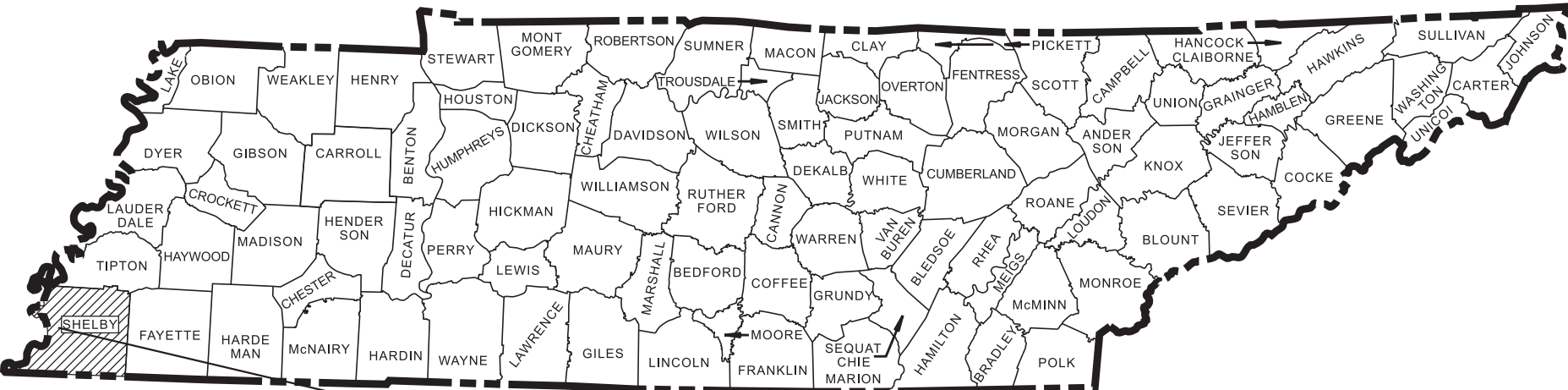


PROJECT LENGTH 5.62 MILES
TOTAL LANE MILES RESURFACED 32.55 MILES

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES X	NO

TENN.	YEAR 2025	SHEET NO. 1
STATE PROJ. NO.	79I240-S8-002	
STATE PROJ. NO.	79I240-M3-003	

REV. 06-23-25 : REMOVED BUNDLING INFORMATION.



PROJECT LOCATION

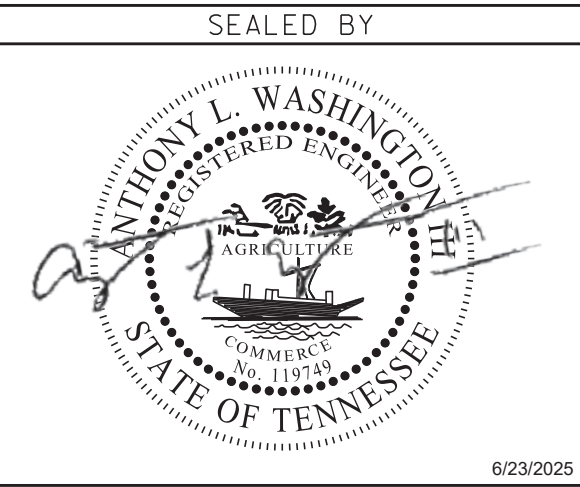
BRIDGE ID. #	79I00400155	79I00400157	79I02400009
	79I02400011	79I02400013	79I02400015
	79I02400017	79I02400019	79I02400023
	79I02400024	79I02400029	79I02400030
	79I02400033	79I02400034	79I02400035
	79I02400036	79I02400037	79I02400038
	79I02400039	79I02400041	79I02400043
	79I02400044	79I02400045	79I02400046
	79I02400047	79I02400048	79I02400049
	79I02400051	79I02400053	79I02400054
	79I02400055	79I02400056	79I02400057
	79I02400061	79I02400063	79I02400065
	79I02400066	79I02400067	79I00550038
	79I00550037	79I00550109	

EXCLUSIONS	
LOG MILE	LENGTH (FT.)
1.713 - 1.96	1302.24
4.474 - 4.606	700.00
TOTAL =	2002.24



TRAFFIC COUNTER, EMBEDDED DETECTION LOOPS & AUTOMATIC TRAFFIC READERS	
STATION LOCATION	LOG MILE
TC STATION 247	0.856
TC STATION 198	2.634
TC STATION 197	4.030
TC STATION 271	4.989
EDL #62 EB	5.280
EDL #61 EB	5.350
EDL #59 WB	5.670
EDL #58 EB	5.680
EDL #552 EB	5.780
EDL #64 EB	5.780
EDL #552 WB	5.820

TRAFFIC DATA	
ADT (2025)	128,633
POSTED SPEED LIMITS	
L.M. 0.00 TO L.M. 6.00	55 MPH



APPROVED: WILL REID, DEPUTY COMMISSIONER / CHIEF ENGINEER

DATE:

APPROVED: HOWARD H. ELEY, DEPUTY GOVERNOR / COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: DIVISION ADMINISTRATOR DATE

ROADWAY INDEX

SHEET NAME SHEET NO.

SIGNATURE SHEETS.....	ROADWAY-SIGN1
SIGNATURE SHEETS.....	ROADWAY-SIGN2
SIGNATURE SHEETS.....	ROADWAY-SIGN3
TITLE SHEET.....	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS AND	
STANDARD TRAFFIC DESIGN DRAWINGS.....	1A
PROJECT COMMITMENTS.....	1B
ESTIMATED ROADWAY QUANTITIES.....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE.....	2B, 2B1-2B4
GENERAL NOTES.....	2C
SPECIAL NOTES	2D
ENVIRONMENTAL NOTES.....	2E, 2E1
TABULATED QUANTITIES.....	2F, 2F1, 2F2
UTILITY NOTES AND UTILITY OWNERS.....	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL....	4
TRAFFIC DETECTION LOOPS DETAIL.....	5, 5-1-5-3
BRIDGE REPAIR PLANS.....	B1

NOTE: THE ALPHABETICAL LETTERS “I”, “O” & “Q” ARE NOT USED IN THE NUMBERING OF SHEETS.

NO UTILITY SHEETS INCLUDED IN THIS SET OF PLANS.

STANDARD ROADWAY DRAWINGS

DWG. REV. DESCRIPTION

STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS

RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
RD-L-3	03-01-23	STANDARD LEGEND FOR SIGNALIZATION AND LIGHTING

CATCH BASINS AND MANHOLES

D-CBB-12A	06-28-19	TYPE 'B' CAST IRON FRAME, GRATE & VERTICAL INLET DETAILS FOR NOS. 10, 12, 14, 16 & 17 TYPE CATCHBASINS
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SAFETY DESIGN AND GUARDRAILS

S-GRS-4	05-04-22	SPECIAL CASE GUARDRAIL HEIGHT TRANSITION DETAIL
S-GRT-1	10-29-21	TYPE 12 GUARDRAIL TERMINAL BURIED-IN-BACKSLOPE
S-GRT-1A		LAYOUT OF FLARED GUARDRAIL (TL- 3)
S-GRT-2	06-28-19	TYPE 38 GUARDRAIL END TERMINAL
S-GRT-2R	06-28-19	EARTH PAD FOR TYPE 38 AND TYPE 21 TERMINAL (RETROFIT)
S-GRA-1	06-28-19	TYPE 12 GUARDRAIL ANCHOR
S-GRA-3	01-09-24	TYPE 13 GUARDRAIL ANCHOR

STANDARD TRAFFIC DESIGN DRAWINGS

DWG. REV. DESCRIPTION

SIGNALS

T-SG-2	06-27-16	LOOP LEAD-INS, CONDUIT AND PULL BOXES
T-SG-3	07-11-17	STANDARD NOTES AND DETAILS OF INDUCTIVE LOOPS
T-SG-3A	06-27-16	ALTERNATE DETECTION DETAILS

PAVEMENT MARKINGS

T-M-3	01-24-25	MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERSAND MEDIANS FOR CONVENTIONAL ROADS
T-M-4	01-24-25	STANDARD INTERSECTION PAVEMENT MARKINGS
T-M-5	01-24-25	MARKING DETAIL FOR FREEWAYS
T-M-6	01-24-25	MARKING DETAIL FOR EXPRESSWAY & FREEWAY INTERCHANGES
T-M-7	01-24-25	GORE MARKING DETAILS FOR EXPRESSWAY & FREEWAY INTERCHANGES
T-M-8	01-24-25	MARKING DETAILS FOR EXPRESSWAYS & FREEWAYS
T-M-9	01-24-25	PAVEMENT MARKINGAND SIGNING DETAILS FOR RAMP INTERSECTIONS
T-M-9A	01-24-25	PAVEMENT MARKINGAND SIGNING DETAILS FOR RAMP INTERSECTIONS
T-M-9B	01-24-25	PAVEMENT MARKINGAND SIGNING DETAILS FOR RAMP INTERSECTIONS
T-M-15	01-24-25	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR INTERSTATE AND ACCESS CONTROLLED ROUTES


WORK ZONES

T-WZ-10	03-26-25	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-11	03-26-25	ONE LANE CLOSURE DETAIL FOR DIVIDED HIGHWAYS
T-WZ-13	03-26-25	TWO-OUTSIDE LANE CLOSURE FOR EXPRESSWAY AND FREEWAYS
T-WZ-18	03-26-25	SHOULDER CLOSURE DETAIL FOR FREEWAYS AND DIVIDED HIGHWAYS
T-WZ-60	03-26-25	FREEWAY RESURFACING SIGNING LAYOUT
T-WZ-63	03-26-25	WORK ZONE IN THE VICINITY OF AN ENTRANCE RAMP
T-WZ-64	03-26-25	WORK ZONE IN THE VICINITY OF AN EXIT RAMP
T-WZ-FAB1	03-26-25	FLASHING YELLOW ARROW BOARD

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	79I240-S8-002	1A

REV. 06-23-25: ADDED SHEET ROADWAY-SIGN2 TO INDEX.
REV. 06-30-25: ADDED SHEET ROADWAY-SIGN3 TO INDEX.
DELETED SHEETS 5 & 5-1. RENUMBERED TRAFFIC DETECTION LOOPS DETAIL SHEETS.

SEALED BY



6/30/2025

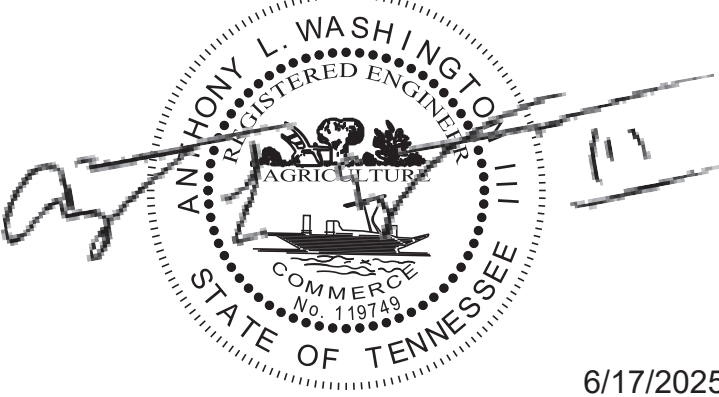
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX,
STANDARD ROADWAY
DRAWINGS, AND
STANDARD TRAFFIC
DESIGN DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	79I240-S8-002	1B

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIRONMENTAL DIVISION HAZARDOUS MATERIALS	ASBESTOS CONTAINING MATERIAL (ACM) SURVEYS WERE COMPLETED ON BRIDGE NO. 79I02400067 I-240 EB OVER RAMP TO I-55 NB LM 5.34 (79-I0240-05.34) AND BRIDGE NO. 79I00550038 I-240 EB OVER I-55 LM 5.48 (79-I0240-05.48R). NO ASBESTOS WAS DETECTED. PLEASE SEE THE REPORT FOR FURTHER DETAILS AND PHOTOGRAPHS. NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03).	BRIDGE NO. 79I02400067 I-240 EB OVER RAMP TO I-55 NB LM 5.34 (79-I0240-05.34) BRIDGE NO. 79I00550038 I-240 EB OVER I-55 LM 5.48 (79-I0240-05.48R)

SEALED BY



6/17/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

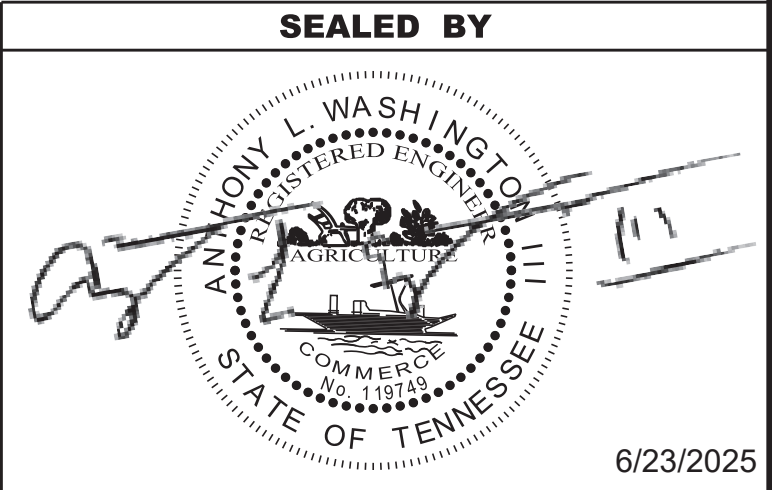
PROJECT
COMMITMENTS

ESTIMATED ROADWAY QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY 791240-S8-002
(1)	202-03.01 REMOVAL OF ASPHALT PAVEMENT	S.Y.	20433
(2)(3)	203-06 WATER	M.G.	7
(4)	208-01.05 BROOMING & DEGRASSING SHOULDERS	L.M.	3.9
(3)(5)	303-02 MINERAL AGGREGATE, TYPE B BASE, GRADING (C OR D)	TON	929
(6)	307-03.01 ASPHALT CONCRETE MIX (PG76-22) (BPMB-HM) GRADING A	TON	1258
(7)	307-03.08 ASPHALT CONCRETE MIX (PG76-22) (BPMB-HM) GRADING B-M2	TON	2000
(8)	403-02.01 TRACKLESS TACK COAT	TON	216
	411-01.07 ACS MIX (PG64-22) GRADING E SHOULDER	TON	8851
(9)(10)	411-01.21 LONGITUDINAL JOINT SEALANT	L.M.	44.9
(11)	411-03.10 ACS MIX(PG76-22) GRADING D	TON	23167
	411-12.01 SCORING SHOULDERS (CONTINUOUS) (16IN WIDTH)	L.M.	23
(12)	415-01.01 COLD PLANING BITUMINOUS PAVEMENT	TON	30967
(13)(14)	611-03.04 GRAY IRON CASTINGS (CATCHBASIN)	LB.	3500
(15)	705-02.10 GUARDRAIL TRANSITION 27IN TO 31IN	EACH	17
(15)	705-04.02 GUARDRAIL TERMINAL (TYPE 12)	EACH	3
(15)	705-04.09 EARTH PAD FOR TYPE 38 GR END TREATMENT	EACH	1
(15)	705-06.10 GR TERMINAL TRAILING END (TYPE 13) MASH TL-3	EACH	12
(15)	705-06.20 TANGENT ENERGY ABSORBING TERM MASH TL-3	EACH	1
(15)	706-01 GUARDRAIL REMOVED	L.F.	330
(16)	712-01 TRAFFIC CONTROL	LS	1
	712-04.01 FLEXIBLE DRUMS (CHANNELIZING)	EACH	838
	712-05.01 WARNING LIGHTS (TYPE A)	EACH	46
	712-05.03 WARNING LIGHTS (TYPE C)	EACH	792
(17)(18)	712-06 SIGNS (CONSTRUCTION)	S.F.	5234
(19)	712-06.16 SIGNS (CONSTRUCTION)(REDUCED SPEED WARNING)	EACH	16
	712-08.03 ARROW BOARD (TYPE C)	EACH	4
	712-08.08 SPEED FEEDBACK SIGN ASSEMBLY	EACH	2
	712-08.09 DIGITAL SPEED LIMIT SIGN ASSEMBLY	EACH	16
	712-08.12 QUEUE PROTECTION TRUCK	DAY	3
	713-16.01 CHANGEABLE MESSAGE SIGN UNIT	EACH	2
(20)(25)	716-01.22 SNOWPLOWABLE RAISED PAVMENT MARKERS (MONO-DIR)(1 COLOR)	EACH	1176
(21)(25)	716-01.23 SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR)	EACH	2386
(22)	716-01.30 REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	2386
(23)(24)(25)	716-02.05 PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	97
(23)(24)(25)	716-02.06 PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	8
(23)(24)(25)	716-02.07 PLASTIC PAVEMENT MARKING (24" BARRIER LINE)	L.F.	3050
(23)(24)(25)	716-02.09 PLASTIC PAVEMENT MARKING (LONGITUDINAL CROSS-WALK)	L.F.	430
(23)(24)(25)	716-03.01 PLASTIC WORD PAVEMENT MARKING (ONLY)	EACH	2
(23)(24)(25)	716-04.01 PLASTIC PAVEMENT MARKING (STRAIGHT-TURN ARROW)	EACH	1
(23)(25)	716-04.04 PLASTIC PAVEMENT MARKING (TRANSVERSE SHOULDER)	L.F.	1606
(23)(25)	716-04.06 PLASTIC PAVEMENT MARKING (WRONG WAY ARROW)	EACH	6
(23)(24)(25)	716-04.07 PLASTIC PAVEMENT MARKING (EXIT ONLY ARROW)	EACH	5
(23)(24)(25)	716-04.12 PLASTIC PAVEMENT MARKING (YIELD LINE)	S.F.	51
(26)	716-05.02 PAINTED PAVEMENT MARKING (8" BARRIER LINE)	L.F.	10504
(26)	716-05.20 PAINTED PAVEMENT MARKING (6" LINE)	L.M.	40
(25)	716-08.03 REMOVAL OF PAVEMENT MARKING (CROSS-WALK)	L.F.	152
(25)	716-08.05 REMOVAL OF PAVEMENT MARKING (STOP LINE)	L.F.	48
(25)(27)	716-12.02 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	40
(23)(25)	716-12.03 ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE)	L.F.	10504
(23)(25)	716-12.05 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN DOTTED LINE)	L.F.	1722
(23)(25)	716-12.09 ENHANCED FLAT LINE THERMO (12IN LINE)	L.F.	4319
(23)(25)	716-12.10 ENHANCED FLAT LINE THERMO (12IN DOTTED)	L.F.	909
	717-01 MOBILIZATION	LS	1
	730-03.20 INSTALL PULL BOX (TYPE A)	EACH	3
	730-03.21 INSTALL PULL BOX (TYPE B)	EACH	2
(29)	730-12.02 CONDUIT 2" DIAMETER (PVC SCHEDULE 40)	L.F.	245
(29)	730-14.02 SAW SLOT	L.F.	1925
(28)(29)	730-14.03 LOOP WIRE	L.F.	5600

FOOTNOTES	
(1)	TO BE USED AS DIRECTED BY THE ENGINEER. INCLUDES 2733 S.Y. FOR BREAKOUTS AND 17700 S.Y. FOR PARTIAL DEPTH ASPHALT REPAIR.
(2)	INCLUDES 3.2 M.G. FOR RAMP REPAIR.
(3)	SEE SHEET 2F1 FOR RAMP/ACCELERATION/DECELERATION (PAVING ITEMS) TABULATION BLOCK FOR RAMP REPAIR LOCATIONS.
(4)	TO BE USED AS DIRECTED BY THE ENGINEER. INCLUDES THE COST OF REMOVING DEBRIS AND SWEEPING SHOULDERS PRIOR TO WORK. SEE SHEET 2C, FINAL PAVEMENT MARKING, NOTE 6 FOR MORE INFORMATION. ITEM TO BE USED AT SPLIT NEAR END OF PROJECT.
(5)	INCLUDES 428.5 TONS FOR RAMP REPAIR.
(6)	TO BE USED FOR BREAKOUT AREAS.
(7)	TO BE USED FOR PARTIAL DEPTH ASPHALT REPAIR.
(8)	INCLUDES 50.9 TONS FOR RAMP REPAIR.
(9)	USE CRAFCO PAVEMENT JOINT ADHESIVE #34524. PAVON JOINT ADHESIVE BY PAVON CORPORATION OR DENSO TAPE BY DENSO.
(10)	TO BE USED FOR SEALING OF ALL SURFACE LAYER CONSTRUCTION JOINTS ALONG THE TRAVEL LANES AND SHOULDERS AS DIRECTED BY THE ENGINEER.
(11)	INCLUDES 7152 TONS FOR RAMP REPAIR.
(12)	INCLUDES 7087 TONS FOR RAMP REPAIR. INCLUDES 9637 TONS FOR SHOULDERS.
(13)	COST TO INCLUDE REMOVAL OF EXISTING GRATE AND REPLACEMENT OF BICYCLE FRIENDLY GRATE WHERE APPLICABLE, INCLUDING MATERIAL, PARTS, LABOR, EQUIPMENT, MACHINERY, TOOLS, OR APPARATUS NECESSARY FOR REMOVAL AND INSTALLATION. THE CONTRACTOR SHALL VERIFY GRATE DIMENSIONS IN THE FIELD.
(14)	SEE STORM DRAIN ADJUSTMENTS TABULATED BLOCK ON SHEET 2F FOR LOCATIONS.
(15)	SEE PROPOSED GUARDRAIL TABULATION BLOCK ON SHEET 2F FOR LOCATIONS.
(16)	ALL SIGNING SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
(17)	THE CONTRACTOR IS RESPONSIBLE FOR THE STAKING OF CONSTRUCTION SIGNS. IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPORARILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL CHOOSE A SIGN COVERING APPROVED BY THE ENGINEER. TEMPORARY SIGN COVERINGS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO 712-06 SIGNS (CONSTRUCTION).
(18)	INCLUDES QUANTITIES ON SHEET 2F.
(19)	ITEM TO BE USED ONLY WHEN A REDUCED SPEED LIMIT IS ESTABLISHED WITHIN THE PROJECT CONSTRUCTION WORK ZONE LIMITS. ITEM INCLUDES SIGN FACE, SUPPORTS, AND TWO TYPE "B" FLASHERS PER THE STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TURNING ON THE TYPE "B" FLASHERS WHEN WORKERS ARE IN THE CONSTRUCTION WORK ZONE AND TURNING THEM OFF WHEN WORKERS ARE NO LONGER IN THE CONSTRUCTION WORK ZONE.
(20)	INCLUDES 154 MARKERS FOR RAMP REPAIR.
(21)	INCLUDES 961 MARKERS FOR RAMP REPAIR.
(22)	FOR REMOVAL OF EXISTING PAVEMENT MARKERS.
(23)	ITEM TO BE USED AS PERMANENT PAVEMENT MARKING.
(24)	THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
(25)	SEE SHEET 2F2 FOR RAMPS/ACCELERATION/DECELERATION (PAVEMENT MARKING ITEMS) TABULATION BLOCK FOR RAMP REPAIR LOCATIONS.
(26)	ITEM TO BE USED AS TEMPORARY PAVEMENT MARKING.
(27)	ITEM TO BE USED AS PERMANENT MARKING FOR EDGELINES AND CENTERLINE.
(28)	ITEM INCLUDES AN EXTRA 200' (20' PER LOOP) THAT IS TO REMAIN IN PULL BOX FOR INSTALLATION BY TRAFFIC COUNTER PERSONNEL.
(29)	SEE SHEET 2F FOR SIGNAL QUANTITIES TABULATION BLOCK FOR LOCATIONS.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	791240-S8-002	2

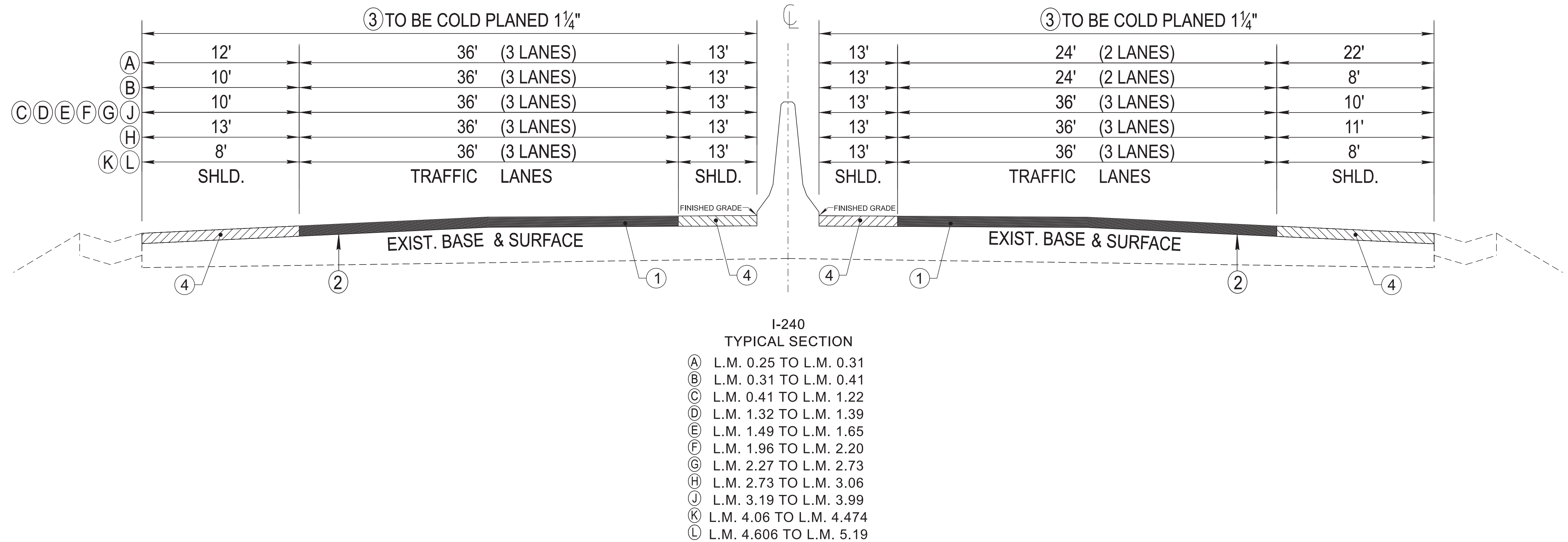
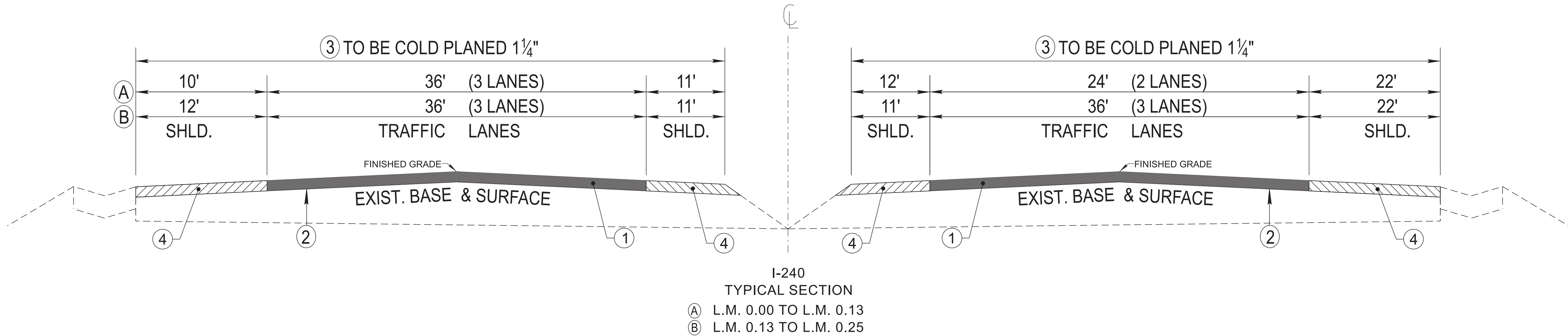
REV. 06-23-25: REVISED QUANTITY FOR ITEMS 712-01 & 717-01



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED
ROADWAY
QUANTITIES

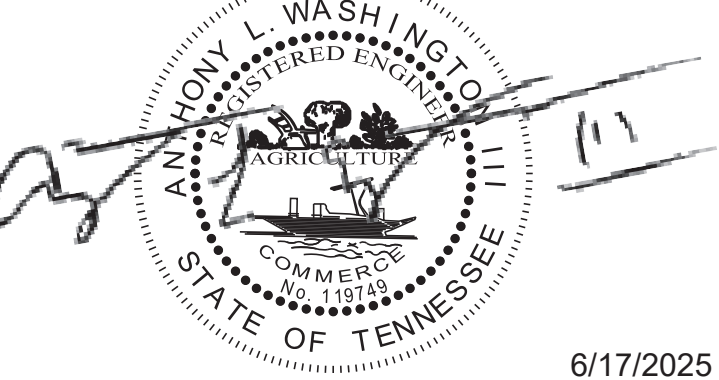
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RESURF	2025	79I240-S8-002	2B



SEE SHEET 2B3 FOR PAVEMENT SCHEDULE

SEE SHEET 2B4 FOR BRIDGE NOTES

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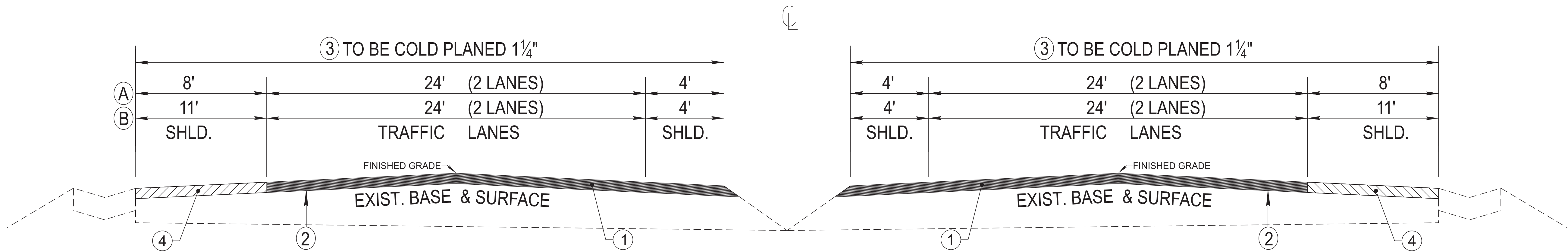
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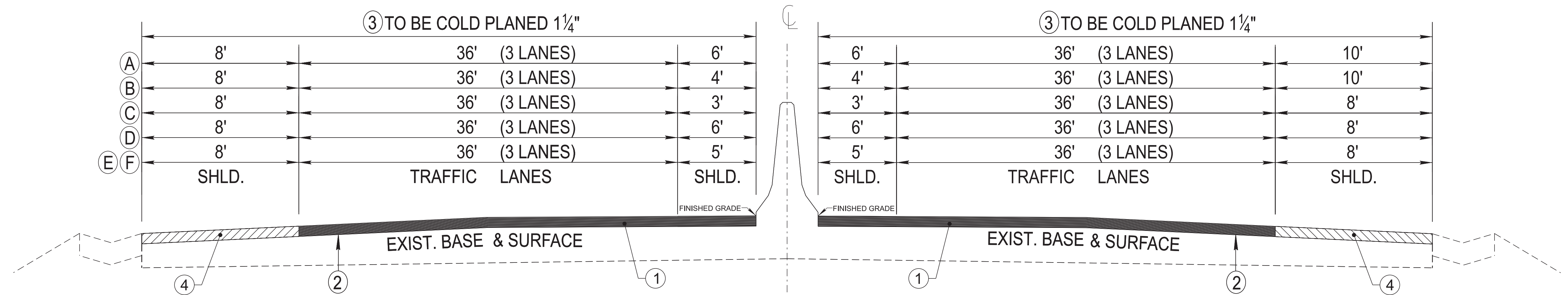
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	79I240-S8-002	2B1



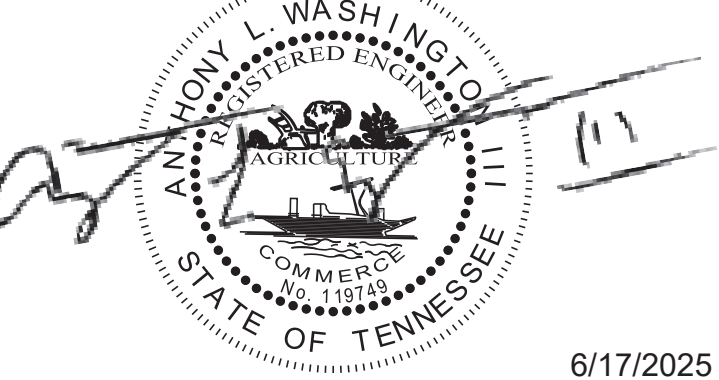
I-240
TYPICAL SECTION
A L.M. 5.64 TO L.M. 5.71
B L.M. 5.71 TO L.M. 6.00



I-240
TYPICAL SECTION
A L.M. 1.39 TO L.M. 1.49
B L.M. 1.65 TO L.M. 1.713
C L.M. 2.20 TO L.M. 2.27
D L.M. 3.99 TO L.M. 4.06
E L.M. 5.19 TO L.M. 5.32
F L.M. 5.36 TO L.M. 5.47

SEE SHEET 2B3 FOR PAVEMENT SCHEDULE

SEE SHEET 2B4 FOR BRIDGE NOTES

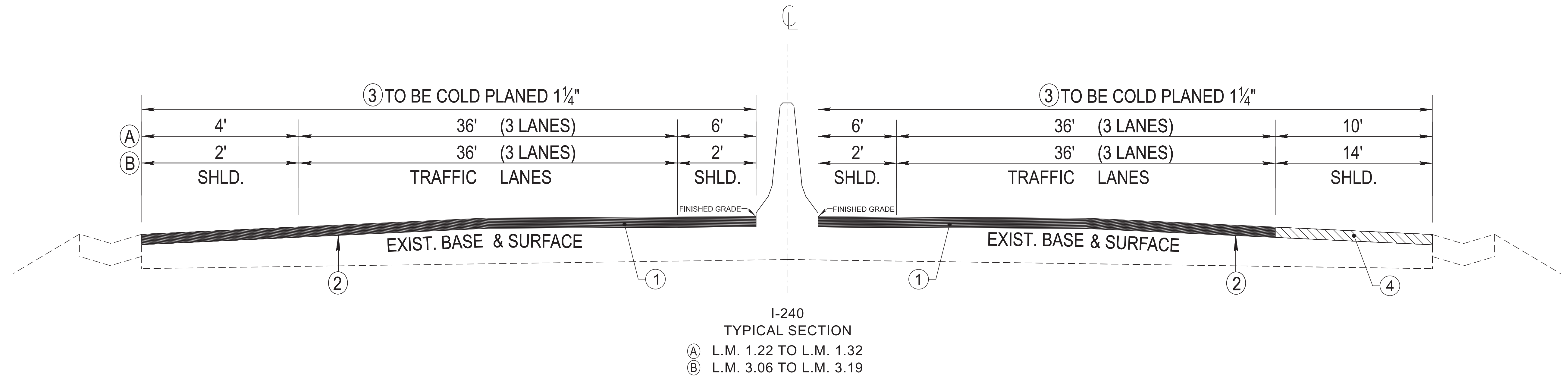
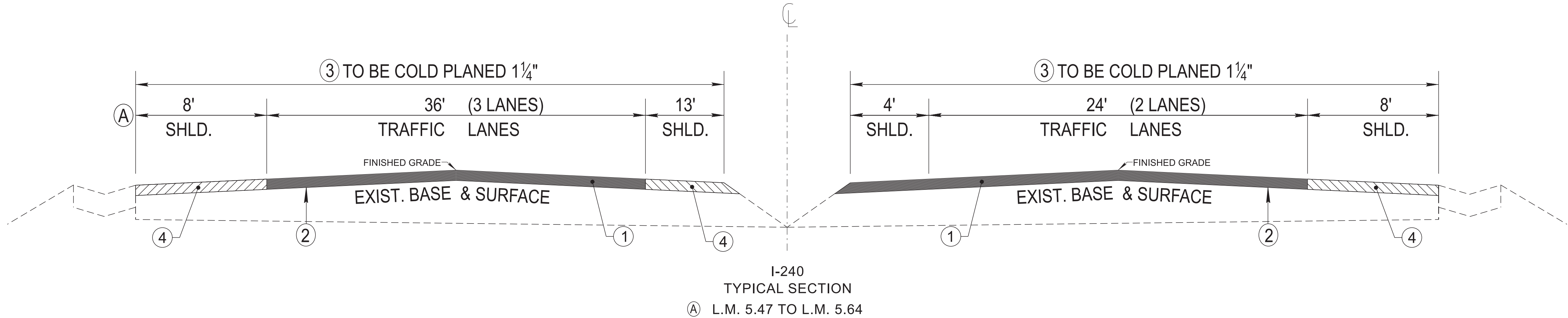
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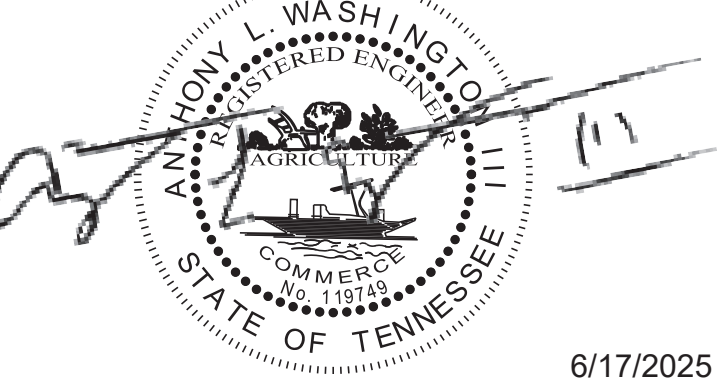
TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	79I240-S8-002	2B2



SEE SHEET 2B3 FOR PAVEMENT SCHEDULE

SEE SHEET 2B4 FOR BRIDGE NOTES

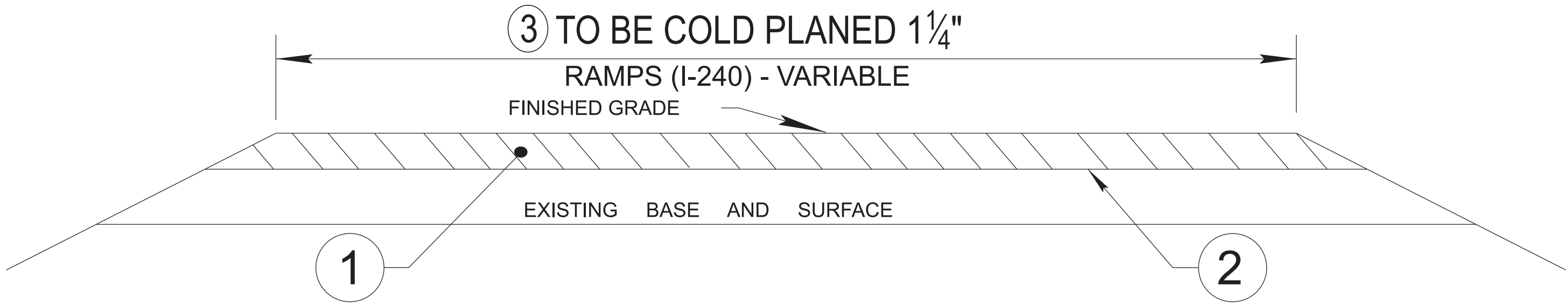
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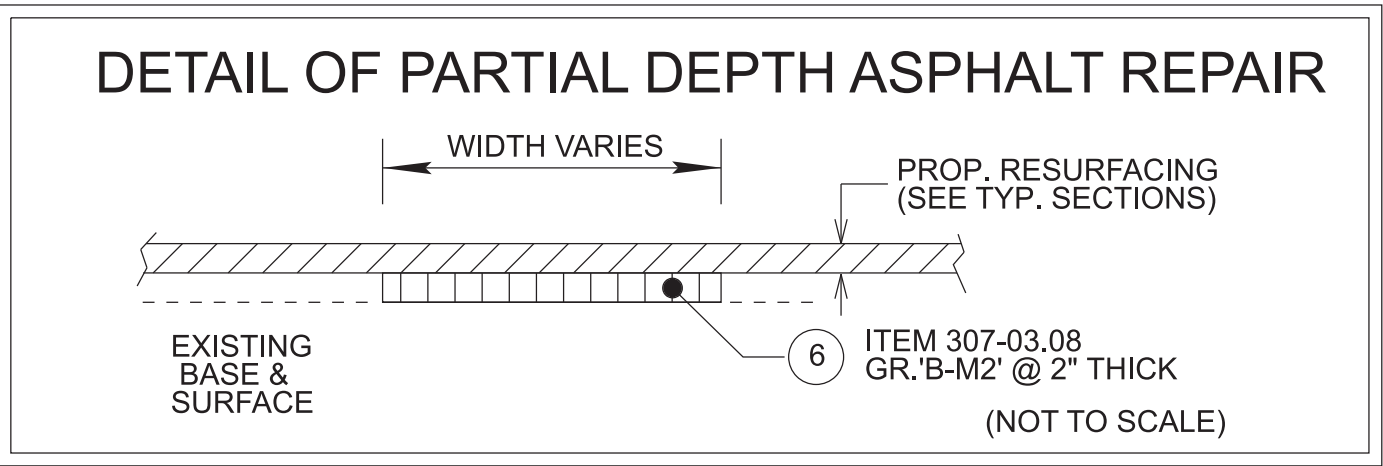
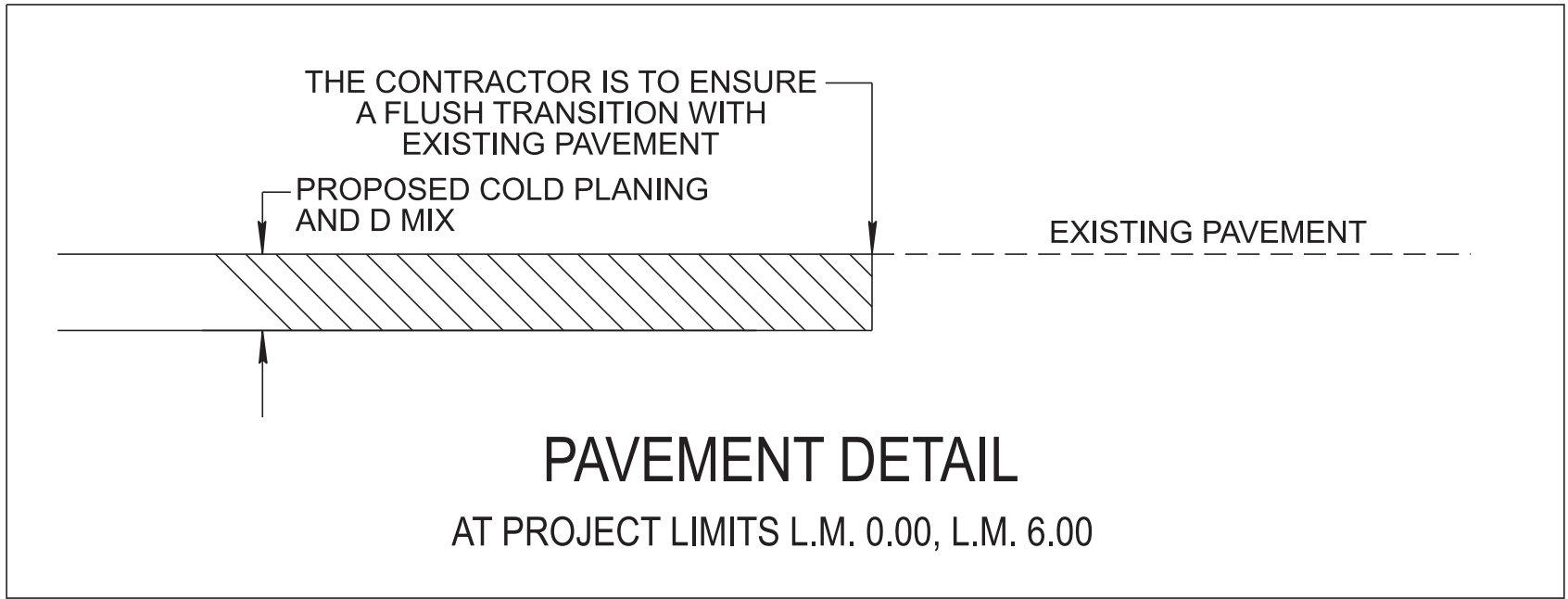
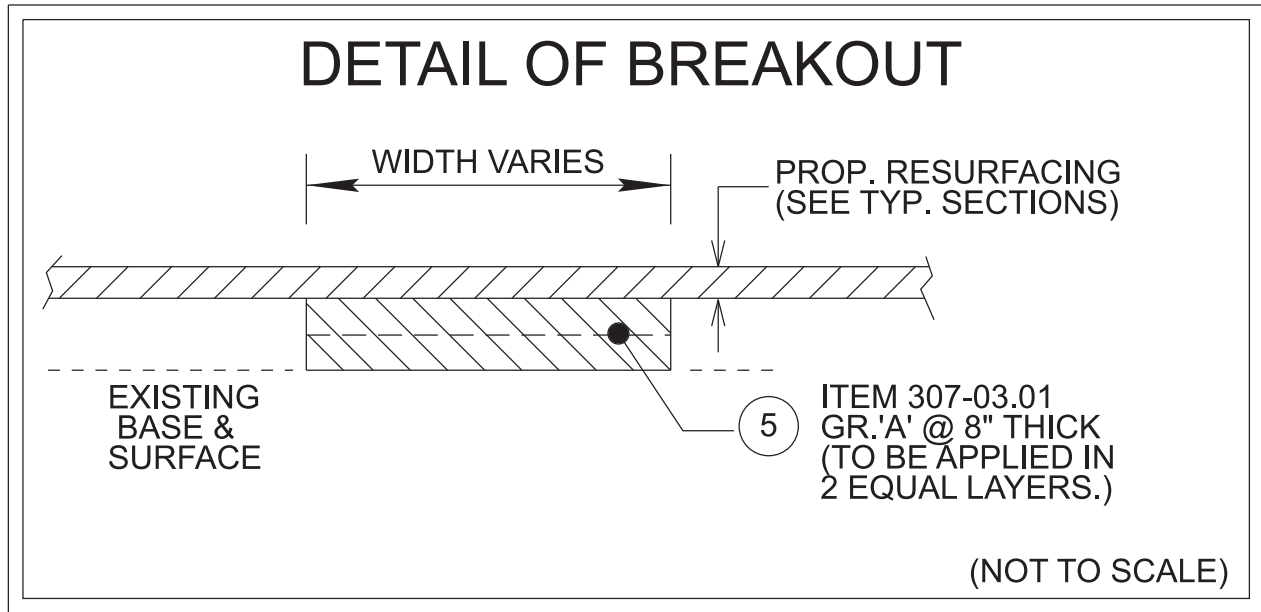
TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

TYPE	YEAR	PROJECT NO.	SHEET NO.
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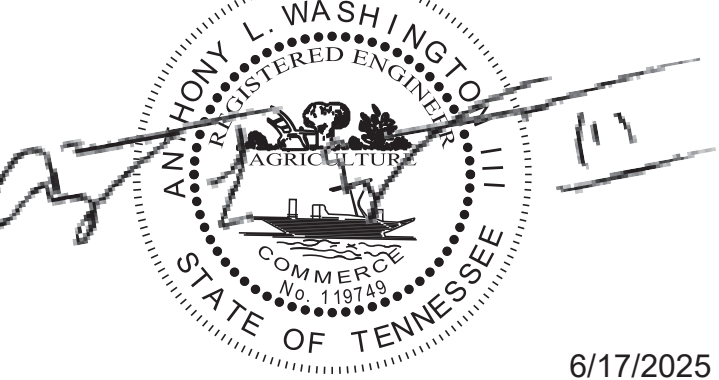


PROPOSED PAVEMENT SCHEDULE

1	ASPHALTIC CONCRETE SURFACE (HM) @ 1.25"± THICK (APPROX. 132.5 LBS./S.Y.) ITEM 411-03.10 ACS MIX (PG76-22) GRADING "D"
2	TRACKLESS TACK COAT ITEM 403-02.01 TRACKLESS TACK COAT (TC) (TON) SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD
3	COLD PLANING @ 1.25"± THICK (APPROX. 131.25 LBS./S.Y.) ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT
4	ASPHALTIC CONCRETE SURFACE (HM) @ 1.25"± THICK (APPROX. 132.5 LBS./S.Y.) ITEM 411-01.07 ACS MIX (PG64-22) GRADING "E" SHOULDER
5	BITUMINOUS COURSE (BLACK BASE) @ 8.00"± THICK (APPROX. 920.0 LBS./S.Y.) ITEM 307-03.01 ASPHALT CONCRETE MIX (PG76-22)(BPMB-HM) GRADING "A" (TO BE USED FOR BREAKOUT ONLY)
6	BITUMINOUS BASE COURSE (BINDER) @ 2.00"± THICK (APPROX. 226.0 LBS./S.Y.) ITEM 307-03.08 ASPHALT CONCRETE MIX (PG76-22)(BPMB-HM) GRADING "B-M2" (TO BE USED FOR PARTIAL DEPTH ASPHALT REPAIR)



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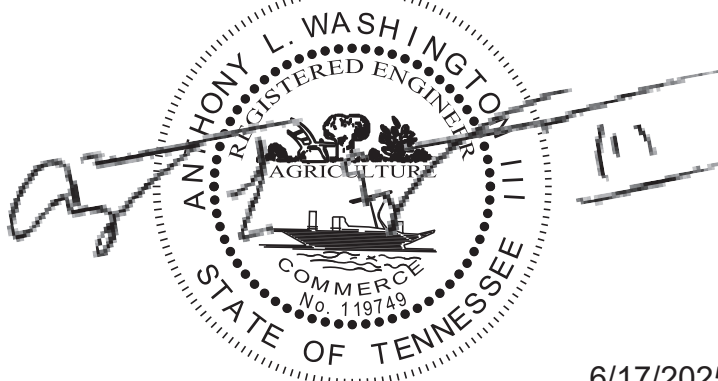


6/17/2025

NOT TO SCALE

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

BRIDGE NOTES (PAGE 1 of 2)					BRIDGE NOTES (PAGE 2 of 2)					TYPE	YEAR	PROJECT NO.	SHEET NO.
BRIDGE NUMBER	LOCATION LOG MILE	CROSSES OVER/UNDER	BRIDGE LENGTH	BRIDGE DECK TREATMENT	BRIDGE NUMBER	LOCATION LOG MILE	CROSSES OVER/UNDER	BRIDGE LENGTH	BRIDGE DECK TREATMENT	RESURF	2025	79I240-S8-002	2B4
79I00400155	0.44	Poplar Ave. (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: NBL: 16'0", SBL: 16'0"	79I02400044	2.91	South Pkwy. E EB Lanes (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: NBL: 16'2", SBL: 16'6"				
79I00400157	0.60	Jefferson Ave. (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: NBL: 17'3", SBL: 18'2"	79I02400045	3.09R	Kerr Ave.	132'	COLD PLANE 1 1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1 1/4" OF NEW ASPHALT.				
79I02400009	0.79	Madison Ave. (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: NBL: 17'5", SBL: 16'4"	79I02400046	3.09L	Kerr Ave.	132'	COLD PLANE 1 1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1 1/4" OF NEW ASPHALT.				
79I02400011	0.86	Ramp from Madison Ave. (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: NBL: 16'8", SBL: 16'5"	79I02400047	3.12R	CSXT/Abandoned RR	168'5 3/4"	COLD PLANE 1 1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1 1/4" OF NEW ASPHALT.				
79I02400013	0.87	Ramp to SR-3 (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: NBL: 16'3", SBL: 16'2"	79I02400048	3.12L	CSXT/Abandoned RR	168' 5 3/4"	COLD PLANE 1 1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1 1/4" OF NEW ASPHALT.				
79I02400015	0.93	SR-3 (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: NBL: 16'6", SBL: 16'6"	79I02400049	3.37	Pedestrian Walkway (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: NBL: 16'1", SBL: 16'2"				
79I02400017	1.04	Eastmoreland Ave. (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: NBL: 16'0", SBL: 15'11"	79I02400051	3.52	Person Ave. (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: NBL: 19'5", SBL: 16'6"				
79I02400019	1.13	Linden Ave. (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: NBL: 18'2", SBL: 18'2"	79I02400053	4.01R	Alice Ave.	121' 1 3/8"	COLD PLANE 1 1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1 1/4" OF NEW ASPHALT.				
79I02400023	1.26R	Peabody Ave.	158'6"	COLD PLANE 1 1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1 1/4" OF NEW ASPHALT.	79I02400054	4.01L	Alice Ave.	121' 1 3/8"	COLD PLANE 1 1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1 1/4" OF NEW ASPHALT.				
79I02400024	1.26L	Peabody Ave.	158'6"	COLD PLANE 1 1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1 1/4" OF NEW ASPHALT.	79I02400055	4.27R	Cane Creek	107' 0 1/4"	COLD PLANE 2 1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1 1/4" OF NEW ASPHALT.				
79I02400029	1.41R	SR-4	219'9 3/8"	COLD PLANE 1 1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1 1/4" OF NEW ASPHALT.	79I02400056	4.27L	Cane Creek	106' 10 7/8"	COLD PLANE 1 1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1 1/4" OF NEW ASPHALT.				
79I02400030	1.41L	SR-4	219'9 3/8"	COLD PLANE 1 1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1 1/4" OF NEW ASPHALT.	79I02400061	4.76	Norris Rd. (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: NBL: 16'9", SBL: 17'8"				
79I02400033	1.68R	Central Ave.	154'0 3/8"	COLD PLANE 3 1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1 1/4" OF NEW ASPHALT.	79I02400063	5.08	Alcy Rd. (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: NBL: 16'3", SBL: 17'0"				
79I02400034	1.68L	Central Ave.	154'0 3/8"	COLD PLANE 3 1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1 1/4" OF NEW ASPHALT.	79I02400065	5.20R	Tarent Branch	53'	PAVE WITH PLANS MIX/TREATMENT TYPE				
79I02400037	2.22R	Walker Ave.	129'3 1/2"	COLD PLANE 3 1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1 1/4" OF NEW ASPHALT.	79I02400066	5.20L	Tarent Branch	31'	PAVE WITH PLANS MIX/TREATMENT TYPE				
79I02400038	2.22L	Walker Ave.	138'7 1/4"	COLD PLANE 3 1/4" OF THE EXISTING ASPHALT AND REPLACE WITH 1 1/4" OF NEW ASPHALT.	79I02400067	5.34	Ramp to I-55/ EB Lanes	123'6"	PROVIDE DECK REPAIR ITEM (BRIDGE SHEETS INCLUDED) REPLACE BRIDGE JOINTS (SEE BRIDGE REPAIR PLANS)				
79I02400039	2.50	E. McLemore Ave. (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: NBL: 16'5", SBL: 16'8"	79I00550038	5.48	I-55 NB Lanes	243' 9 1/4"	PROVIDE DECK REPAIR ITEM (BRIDGE SHEETS INCLUDED) REPLACE BRIDGE JOINTS (SEE BRIDGE REPAIR PLANS)				
79I02400041	2.77	Trigg Ave. (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: NBL: 16'4", SBL: 16'2"	79I00550037	5.58	I-55 NB LNS. & I-240 WB LNS. (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: EBL: 17'0", WBL: 16'3"				
79I02400043	2.90	South Pkwy. E WB Lanes (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: NBL: 16'2", SBL: 16'6"	79I00550109	5.840	SR-3 (UNDERPASS)	N/A	TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT VERTICAL CLEARANCE: EBL: 16'1", WBL: 17'3"				
										<div>SEALED BY</div> <div></div> <div>6/17/2025</div>			
										NOT TO SCALE			
										STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION			
										TYPICAL SECTIONS AND PAVEMENT SCHEDULE			

GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

GUARDRAIL

- (1) THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPES UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.
- (3) IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS TO DELINEATE GUARDRAIL END AND A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL END TERMINAL.

MISCELLANEOUS

- (7) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6" LINE), L.M.

FINAL PAVEMENT MARKING

- (6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

A. BROOMING & DE-GRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OF ROADWAY (INCLUDING PAVED AND GRAVEL SHOULDERS), AS DIRECTED BY THE ENGINEER.

B. ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE ENGINEER.

C. ALL COSTS ASSOCIATED WITH PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.

D. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

- (8) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.
- (9) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 8" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.03, ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE), L.F. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (20) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVEMENT

PAVING

- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING

- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

SIGNALIZATION

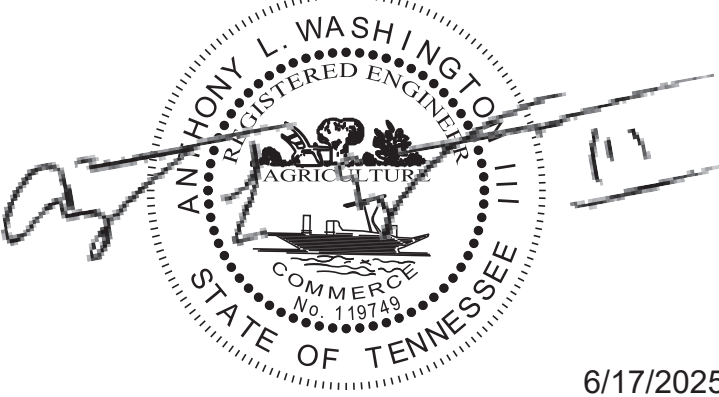
- (1) EQUIPMENT AND INSTALLATION OF TRAFFIC SIGNALS SHALL COMPLY WITH TDOT STANDARD SPECIFICATIONS, SECTION 730.
- (7) DETECTION LOOPS SHALL BE INSTALLED BEFORE THE FINAL SURFACE IS APPLIED.
- (11) THE PROJECT ENGINEER SHALL NOTIFY THE LOCAL GOVERNMENTAL AGENCY RESPONSIBLE FOR TRAFFIC CONTROL MAINTENANCE AT LEAST ONE DAY IN ADVANCE OF THE COLD PLANING ACTIVITY AT SIGNALIZED INTERSECTIONS WHERE DETECTOR LOOPS ARE ON THE PAVEMENT. THE MAINTAINING AGENCY WILL THEN BE RESPONSIBLE FOR DISCONNECTING THE LOOP DETECTORS AND MAKING ANY NECESSARY TIMING ADJUSTMENTS IN THE SIGNAL CONTROLLER PRIOR TO THE CONSTRUCTION.
- (14) LOOP REPLACEMENT SHALL BE IN ACCORDANCE WITH SECTION 730 OF THE STANDARD SPECIFICATIONS.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADTS LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

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GENERAL
NOTES

SPECIAL NOTES

RESURFACING

- (1)

TRAFFIC WILL BE ALLOWED TO TEMPORARILY DRIVE ON THE MILLED SURFACE OF THE ROADWAY UNDER THE FOLLOWING CONDITIONS ONLY:

a.

THE MILLED SURFACE IS FINE TEXTURED. THE FINE TEXTURE SHALL BE OBTAINED BY A MILLING MACHINE UTILIZING A MILLING HEAD WITH TEETH SPACING 3/8" OR LESS OPERATING AT LESS THAN 80 FEET PER MINUTE.

b.

THE SURFACE SHALL BE SWEEPED AND CLEANED OF ALL LOOSE MATERIALS.

c.

THE MILLED SURFACE SHALL BE PAVED WITHIN 72 HOURS IF THE CURRENT ADT IS ≥ 70,000 OR WITHIN 96 HOURS IF THE CURRENT ADT IS < 70,000.

d.

RAIN OR INCLEMENT WEATHER IS NOT EXPECTED OR FORECASTED WITHIN 48 HOURS AFTER MILLING.

e.

ALL APPLICABLE SIGNING IS INSTALLED IN ACCORDANCE WITH THE MUTCD. SIGNING SHALL INCLUDE MOTORCYCLE WARNING SIGNS (W8-15 AND W8-15P) PLACED IN ADVANCE OF ANY MILLED AREAS.

f.

IF MILLED SURFACE BEGINS TO DETERIORATE, PAVING TO COVER UP DETERIORATING MILLED SURFACES SHOULD OCCUR AS DIRECTED BY THE ENGINEER DURING THE NEXT WORKING DAY. IF SEVERE DISTRESS OCCURS, IMMEDIATE RESPONSE WILL BE REQUIRED.

g.

ONLY ONE LANE IN EACH DIRECTION SHALL HAVE A MILLED SURFACE AT ONE TIME.
- (2)

SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER. "
- (3)

PAVE CONCURRENTLY WHERE SHOULDER AND ADJACENT LANES ARE DESIGNATED TO BE PAVED WITH THE SAME MIX.
- (4)

THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF BRIDGE DECK SO AS NOT TO DAMAGE THE EXISTING DECK SEALANT AND/OR EXPANSION JOINT MEMBERS (STEEL PLATES, BARS, AND/OR HEADERS). IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS TO ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.
- (8)

FEATHER SURFACE MIX TO ENDS OF BRIDGES THAT ARE NOT TO BE PAVED.

STORM DRAIN STRUCTURES

- (1)

TAPER AROUND ALL CATCH BASINS AS DIRECTED BY THE ENGINEER.

PAVEMENT MARKING

- (7)

UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.20.
- (8)

UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-12.02.

SIGNS

- (9)

IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION, THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

MISCELLANEOUS

- (1)

ITEM 303-02 TO BE PLACED BEFORE PLACING SURFACING MATERIAL.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1)

THE CONTRACTOR SHALL KEEP TWO TRAFFIC LANES, ONE IN EACH DIRECTION, OPEN TO TRAFFIC DURING NON-WORK HOURS OR NON-WORK DAYS.

- (2)

THE DEPARTMENT SHALL RESERVE THE RIGHT TO REOPEN LANES AS NECESSARY WHEN TRAFFIC CONDITIONS ARE DEEMED UNACCEPTABLE (EXCESSIVE QUEUE LENGTHS AND DELAY TIMES). THE CONTRACTOR SHALL BE REQUIRED TO FULLY COOPERATE WITH THE PROJECT SUPERVISOR WHEN REQUESTED TO MAKE CHANGES TO THE TRAFFIC CONTROL.
- (3)

MESSAGE BOARDS SHALL BE USED NEAR INTERCHANGES AND/OR OTHER DESIGNATED AREAS IN ADVANCE OF THE WORK ZONE TO ALERT MOTORISTS OF POSSIBLE DELAYS AND RECOMMEND THE USE OF ALTERNATE ROUTES. THE MESSAGES SHALL BE UPDATED AS OFTEN AS NECESSARY SO THAT THE MOTORISTS OBTAIN CURRENT TRAFFIC INFORMATION. MESSAGE BOARDS SHALL BE RELOCATED AS NECESSARY AS THE WORK PROGRESSES. THE CONTRACTOR SHALL BE REQUIRED TO IDENTIFY AN INDIVIDUAL WORKING ON THE PROJECT THAT WILL BE RESPONSIBLE FOR KEEPING THE MESSAGES CURRENT AND RELOCATING MESSAGE BOARDS AS REQUESTED BY TDOT. THE PROJECT SUPERVISOR SHALL HAVE THE AUTHORITY TO APPROVE ALL MESSAGES AND REQUIRED CHANGES AT ANY TIME DUE TO CHANGING TRAFFIC CONDITIONS.

JOINT SEALANTS

- (3)

THE CONTACT SURFACE OF TRANSVERSE JOINTS AND LONGITUDINAL JOINTS IN THE SURFACE LIFT SHALL BE SEALED BY APPLYING JOINT SEALANT PRIOR TO PLACEMENT OF ADDITIONAL ASPHALT AGAINST THE PREVIOUSLY PLACED MATERIAL. MANUFACTURER'S RECOMMENDATIONS SHALL BE FOLLOWED IF THE MATERIAL NEEDS TO BE RE-HEATED, AND WHEN PLACING THE THIN, UNIFORM COAT.
- (4)

PRIOR TO APPLICATION OF THE SEALANT, THE FACE OF THE JOINT SHALL BE THOROUGHLY DRY AND FREE FROM DUST OR ANY OTHER MATERIAL THAT WOULD PREVENT PROPER SEALING. ALL JOINTS SHALL BE SWEEPED OR BLOWN FREE OF LOOSE MATERIAL DIRT, VEGETATION, AND OTHER DEBRIS BY MEANS OF COMPRESSES AIR OR A POWER SWEEPER.
- (5)

TRUCK AND VEHICLE TRAFFIC SHALL NOT DRIVE ACROSS A SEALED JOINT UNTIL IT HAS DURED SUFFICIENT TO PREVENT DAMAGE FROM TRACKING.

SPECIAL TRAFFIC COUNTER SPECIFICATIONS

- (1)

THE CONTRACTOR SHALL SUPPLY AND INSTALL ALL RELATED EQUIPMENT. THE CONTRACTOR SHALL MAKE SURE EACH DETECTION LOOP IS OPERATIONAL AT THE COMPLETION OF THE PROJECT. THE STATE'S PROJECT PLANNING DIVISION'S TRAFFIC COUNTER PERSONNEL WILL MAKE THE TRAFFIC COUNTER FULLY OPERATIONAL.
- (2)

ALL LOOPS SHALL BE FIVE FEET BY SEVEN FEET (5' X 7') AND TYPICAL IN SIZE WITH THREE (3) TURNS OF LOOP WIRE.
- (3)

LEAD WIRES TO ALL LOOPS SHALL BE RUN THROUGH 2-INCH CONDUIT FROM THE SHOULDER OF THE ROADWAY TO AN IN-GROUND PULL BOX. THE PULL BOX SHOULD BE LOCATED AS CLOSE AS POSSIBLE AND BEHIND THE GUARDRAIL. LOOP LEAD-INS AND CONDUIT SHALL BE MARKED ON THE ROADWAY SURFACE SO THAT ANY GUARDRAIL POST INSTALLATION OR OTHER CONSTRUCTION WILL NOT DISTURB THIS UNDERGROUND WORK.
- (4)

AN EXTRA TWENTY FEET (20') OF LOOP WIRE FOR EACH OF THE LOOPS SHALL BE STORED IN THE PULL BOX FOR FUTURE INSTALLATION BY TRAFFIC COUNTER PERSONNEL. A PULL STRING SHALL BE INCLUDED IN THE STUB OUT CONDUIT.
- (5)


EACH LOOP DETECTOR LEAD-IN SHALL BE MARKED WITH LANE IDENTIFICATION, DENOTING THE LANE AND POSITION IN THE LANE FOR EACH LOOP (SUCH AS EB-1 FOR EASTBOUND 1 OR RAMP SB-1, ETC.).
- (6)

FOR QUESTIONS CONCERNING ANY OF THE INSTALLATION OF THE COUNTER EQUIPMENT, PLEASE CONTACT STANLEY DUNN AT 615-350-4571 OR AT STANLEY.DUNN@TN.GOV. CONTRACTOR SHALL NOTIFY STANLEY DUNN TO REPORT THAT WORK HAS BEEN COMPLETED SO THAT HE CAN FINISH THE INSTALLATION.
- (7)

THE CONTRACTOR SHALL SUPPLY STANLEY DUNN WITH THE X / Y COORDINATES (BASED ON WGS COORDINATES) OF EACH PULL BOX.

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SPECIAL
NOTES

ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES(E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ENGINEERING PRODUCTION SUPPORT DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

PROJECT COMMITMENTS

- (5) SEE PROJECT COMMITMENTS, SHEET 1B, FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

SCOPE OF WORK

- (6) THIS PROJECT INCLUDES RESURFACING I-240 AND RAMPS, STRIPING, TRAFFIC CONTROL, BRIDGE REPAIR, AND GUARDRAIL WORK.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.


GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.

- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

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STATE OF TENNESSEE
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ENVIRONMENTAL
NOTES

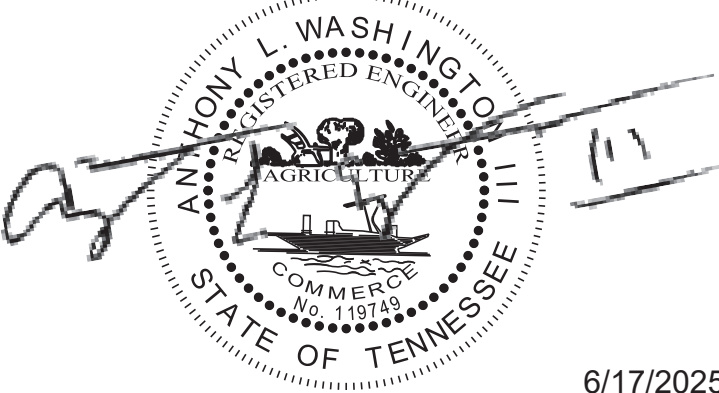
ENVIRONMENTAL NOTES (CONT.)

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

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RESURF	2025	79I240-S8-002	2E1

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ENVIRONMENTAL
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TRAFFIC CONTROL SIGN TABULATION (RESURFACING)					
M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES L x W	S.F.	TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.
G20-1	ROAD WORK NEXT 6 MILES	48" x 24"	8	4	32
G20-2	END ROAD WORK	48" x 24"	8	18	144
R2-1	SPEED LIMIT	36" x 48"	12	26	312
W8-11	UNEVEN LANES	48" x 48"	16	78	1248
W20-1	ROAD WORK 1 MILE	48" x 48"	16	4	64
W20-1	ROAD WORK 1/2 MILE	48" x 48"	16	4	64
W20-1	ROAD WORK 1000 FT	48" x 48"	16	4	64
W20-1	ROAD WORK AHEAD	48" x 48"	16	18	288
W8-15	GROOVED PAVEMENT	48" x 48"	16	38	608
W8-15P	MOTORCYCLE PLAQUE	30" x 24"	5	38	190
W20-5R	RIGHT LANE CLOSED 1/2 MILE	48" x 48"	16	4	64
W20-5R	RIGHT LANE CLOSED 1500 FT	48" x 48"	16	4	64
W4-2R	RIGHT LANE ENDS	48" x 48"	16	8	128
W4-1L	MERGE LEFT ARROW	48" x 48"	16	8	128
W4-1R	MERGE RIGHT ARROW	48" x 48"	16	14	224
W20-5L	LEFT LANE CLOSED 1/2 MILE	48" x 48"	16	4	64
W20-5L	LEFT LANE CLOSED 1500 FT	48" x 48"	16	4	64
W20-5R	RIGHT TWO LANES CLOSED 1/2 MILE	48" x 48"	16	4	64
W20-5R	RIGHT TWO LANES CLOSED 1500 FT	48" x 48"	16	4	64
W20-5L	LEFT TWO LANES CLOSED 1/2 MILE	48" x 48"	16	4	64
W20-5L	LEFT TWO LANES CLOSED 1500 FT	48" x 48"	16	4	64
W4-2L	LEFT LANE ENDS	48" x 48"	16	8	128
W3-2	YIELD AHEAD	48" x 48"	16	14	224
R1-2	YIELD	48" x 48"	16	14	224
E5-2	EXIT OPEN	48" x 36"	12	14	168
E5-1	EXIT	72" x 60"	30	14	420
W21-2	FRESH OIL - PORTABLE	48" x 48"	16	4	64
THIS CONSTRUCTION SIGNING IS TO BE A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.				TOTAL	5234


STORM DRAIN ADJUSTMENTS (RESURFACING)		
RAMP NAME	LT	RT
I-240 SB OFF RAMP AT EXIT 30 TO MADISON AVE. EB INTERSECTION NEAR BRIDGE	X	
MADISON AVE. WB TO I-240 NB ON RAMP NEAR MADISON AVE. GORE AREA		X
MADISON AVE. EB TO I-240 NB ON RAMP NEAR CROSSWALK		X
SR-3 EB ON RAMP TO I-240 SB NEAR CROSSWALK	X	
US-78 (LAMAR AVE.) WB TO I-240 SB ON RAMP AFTER CROSSWALK		X
US-78 (LAMAR AVE.) EB TO I-240 SB ON RAMP AFTER CROSSWALK		X
US-78 (LAMAR AVE.) EB TO I-240 SB ON RAMP AFTER CROSSWALK	X	
US-78 (LAMAR AVE.) EB TO I-240 SB ON RAMP AFTER CROSSWALK		X
I-240 NB OFF RAMP AT EXIT 29 TO US-78 (LAMAR AVE.) EB BEFORE CROSSWALK		X
I-240 NB OFF RAMP AT EXIT 29 TO US-78 (LAMAR AVE.) EB BEFORE CROSSWALK		X
S PARKWAY E WB TO I-240 SB ON RAMP BEFORE CROSSWALK		X
I-240 NB OFF RAMP AT EXIT 28B TO S PARKWAY E WB BEFORE CROSSWALK		X
S PARKWAY E EB TO I-240 NB ON RAMP BEFORE CROSSWALK		X
CUMMINGS ST. NB TO I-240 NB ON RAMP AT SIGNALIZED INTERSECTION CROSSWALK	X	

SIGNAL QUANTITIES TABULATION BLOCK			
INTERSECTION	730-12.02	730-14.02	730-14.03
	CONDUIT 2" DIAMETER (PVC)	SAW SLOT	LOOP WIRE
	(L.F.)	(L.F.)	(L.F.)
MADISION AVE. (@I-240 SB EXIT RAMP)	50	250	450
LAMAR AVE./US 78 (@ I-240 SB EXIT RAMP)	60	275	450
TOTAL	110	525	900

PROPOSED GUARDRAIL (RESURFACING)											
SIDE				LOG MILE	GUARDRAIL			TERMINAL ANCHORS			NOTES
					GUARDRAIL TRANSITION 27 IN TO 31 IN	EARTH PAD FOR TYPE 38 GR. END TREATMENT	GUARDRAIL REMOVED	TYPE 12	TYPE 13	TYPE 38	
EB		WB			705-02.10 (EACH)	705-04.09 (EACH)	706-01 (L.F.)	(21.875') 705-04.02 (EACH)	MASH TL3 (9.375') 705-06.10 (EACH)	MASH TL3 (26.896') 705-06.20 (EACH)	
LT	RT	LT	RT								
			X	0.832	1		10		1		MADISON AVE EB TO I-240 WB ON RAMP
			X	1.059	1		10		1		(EXIT 40) I-240 WB OFF RAMP TO UNION AVE
			X	1.156	1		10		1		(EXIT 40) I-240 WB OFF RAMP TO UNION AVE
	X			2.109	1		10		1		
			X	2.275	1		10		1		
			X	2.53	1		50	1			
			X	3.381	1		50	1			
			X	3.804	1		10		1		
			X	4.776	1		50	1			
	X			5.188	1		10		1		
	X			5.411	1		10		1		
		X		5.453	2		20		1		
X				5.516	1		10		1		
X				5.622	1	1	50			1	
			X	5.636	1		10		1		
			X	5.901	1		10		1		
TOTALS					17	1	330	3	12	1	

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TABULATED
QUANTITIES

RAMP, ACCELERATION LANE, AND DECELERATION LANE TABULATION (PAVING ITEMS)														
	EXIT #	DIRECTION				LOG MILE (L.M.)		203-06	303-02	403-02.01	411-03.10	415-01.01		
		N.B.		S.B.				WATER	MINERAL AGGREGATE (C OR D)	TRACKLESS TACK COAT	ACS MIX GRADING D (PG76-22)	COLD PLANING		
		LT.	RT.	LT.	RT.	BEGIN	END	(M.G.)	(TONS)	(TONS)	(TONS)	(TONS)		
DECELERATION EXIT ONLY LANE TO EXIT 30	30				X	0.00	0.220			0.67	103	102		
I-240 EXIT 30 TO MADISON AVE	30				X			0.12	16	5.00	784	777		
I-40 EB EXIT FOR MADISON THAT TIES IN TO SB I-240 EXIT 30	30				X		0.220	0.07	10	1.71	261	258		
EXIT (1E) JACKSON, MS (FROM EB I-40 TO SB I-240)	1E				X		0.394	0.31	42	3.39	519	514		
WB MADISON AVE ON RAMP TO I-240 NB	N/A			X		0.117 AND 0.782		0.06	8	0.62	94	93		
EXIT ONLY LANE	30			X		0.00	0.117			0.34	52	51		
EB MADISON AVE ON RAMP TO I-240 NB	N/A			X			0.782	0.26	35	3.85	588	583		
WB UNION AVE TO I-240 SB ENTRANCE	N/A				X		0.925	0.19	25	1.50	229	227		
EB UNION AVE TO I-240 SB ENTRANCE	N/A				X		1.24			2.05	314	311		
TAPERED ACCELERATION LANE	N/A				X	1.24	1.32			0.61	13	13		
I-240 NB EXIT RAMP TO WB UNION AVENUE	30		X				1.423	0.36	48	4.85	742	735		
I-240 NB EXIT RAMP TO EB UNION AVENUE	30		X				1.069	0.10	13	1.01	154	153		
PARALLEL ACCELERATION LANE	N/A			X		1.22	1.27			0.87	38	37		
EB LAMAR AVE TO NB I-240	N/A			X			1.27	0.06	8.5	1.64	250	248		
WB LAMAR AVE TO NB I-240	N/A			X			1.433	0.06	8.5	0.45	69	68		
I-240 NB EXIT TO LAMAR AVE	29		X					0.13	17					
I-240 NB EXIT TO LAMAR AVE EB	29		X				1.627	0.27	36	1.03	158	156		
SB I-240 EXIT TO LAMAR AVE	29				X		1.072			2.44	373	373		
EB LAMAR ENTRANCE TO SB I-240	N/A				X		1.398	0.05	6.5	0.52	79	78		
PARALLEL ACCELERATION LANE	N/A				X	1.398	1.490			0.21	20	20		
WB LAMAR AVE ENTRANCE TO SB I-240	N/A				X		1.575	0.18	23.5	0.95	132	131		
PARALLEL ACCELERATION LANE	N/A				X	1.575	1.663			0.78	18	18		
DECELERATION LANE	28B				X		2.656			0.45	8	8		
SB I-240 EXIT TO COLLEGE ST	28B				X		2.712			0.81	124	123		
PARALLEL ACCELERATION LANE	N/A			X		2.673	2.767			0.62	27	27		
CUMMINGS ST EXIT TO NB I-240	N/A			X			2.767			0.40	61	61		
NB I-240 EXIT TO WB SOUTH PARKWAY	28B			X			2.854	0.11	15	0.74	114	113		
AUXILIARY LANE	28B			X			2.854			0.22	34	34		
EB SOUTH PARKWAY TO NB I-240	N/A			X			2.932	0.10	14	0.64	98	97		
WB SOUTH PARKWAY EXIT TO SB I-240	N/A				X		2.861	0.06	8.5	0.59	91	90		
AUXILIARY LANE	28A				X		2.861			0.21	33	32		
I-240 SB EXIT RAMP TO EB S PKWY E	28A				X		2.936	0.12	16	0.69	105	104		
NB I-240 EXIT RAMP TO CUMMINGS ST	28A		X				3.022			0.33	13	13		
DECELERATION LANE	28A			X			3.022			0.33	13	12		
SB I-240 ENTRANCE RAMP FROM COLLEGE ST	N/A				X		3.046			0.45	68	68		
PARALLEL ACCELERATION LANE	N/A				X		3.046			0.95	50	49		
SB I-240 EXIT TO NORRIS ROAD	26				X		4.585	0.13	17	0.91	139	138		
PARALLEL ACCELERATION LANE	N/A			X			4.606			0.28	13	13		
WB NORRIS ROAD EXIT TO NB I-240	N/A			X			4.650			0.53	81	81		
EB NORRIS ROAD EXIT TO SB I-240	N/A				X		4.849	0.06	8	0.52	79	78		
PARALLEL ACCELERATION LANE	N/A				X		4.849			0.15	23	23		
NB I-240 EXIT TO HERNANDO ROAD	26		X				5.179	0.18	24	0.74	113	113		
DECELERATION EXIT ONLY LANE TO HERNANDO ROAD	26		X				5.179			0.48	74	73		
SB I-240 EXIT TO I-55 NORTH ST. LOUIS EXIT 25B	25B				X		5.211			1.47	226	223		
I-240 NB ON RMAP ONLY	N/A			X			5.349	0.22	29	2.03	310	307		
EXIT ONLY LANE	6B				X	5.511	5.71			0.61	93	92		
EXIT ONLY LANE	24			X		5.71	6.00			1.12	171	169		
TOTAL								3.2	428.5	51	7152	7087		

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TABULATED
QUANTITIES

RAMP, ACCELERATION LANE, AND DECELERATION LANE TABULATION (PAVEMENT MARKING ITEMS)																									
	EXIT #	DIRECTION				LOG MILE (L.M.)	716-01.22	716-01.23	716-02.05	716-02.06	716-02.07	716-02.09	716-03.01	716-04.01	716-04.06	716-04.07	716-04.12	716-08.03	716-08.05	716-12.02	716-12.03	716-12.05	716-12.09	716-12.10	
		N.B.		S.B.			MONO DIRECTIONAL SNOWFLOWABLE	SPPM (BI-DIR) (2 COLOR)	STOP BAR	TURN LANE ARROW	24" CHEVRONS FOR GORE AREAS	LONGITUDINAL CROSSWALK	ONLY	STRAIGHT-TURN ARROW	WRONG WAY ARROW	EXIT ONLY ARROW	YIELD LINE	REMOVAL OF PAVEMENT MARKING (CROSS-WALK)	REMOVAL OF PAVEMENT MARKING (STOP LINE)	6" LINE	8" BARRIER LINE FOR GORE AREA	6" DOTTED LINE	12" LINE	12" DOTTED LINE	
		LT.	RT.	LT.	RT.	BEGIN																			END
DECELERATION EXIT ONLY LANE TO EXIT 30	30				X	0.00	0.220			36						1							500	162	
I-240 EXIT 30 TO MADISON AVE	30				X			28		56	24		4	314	54	2					1.31	334		370	
I-40 EB EXIT FOR MADISON THAT TIES IN TO SB I-240 EXIT 30	30				X	0.220				21				68							0.47	418	38		
EXIT (1E) JACKSON, MS (FROM EB I-40 TO SB I-240)	1E				X	0.394				19				40							0.68	334			
WB MADISON AVE ON RAMP TO I-240 NB	N/A			X		0.117 AND 0.782				25		15									0.17	507			
EXIT ONLY LANE	30			X		0.00	0.117			10														149	
EB MADISON AVE ON RAMP TO I-240 NB	N/A			X		0.782				13				59	26						0.88	215			
WB UNION AVE TO I-240 SB ENTRANCE	N/A				X	0.925															0.39				
EB UNION AVE TO I-240 SB ENTRANCE	N/A				X	1.24				36				113							0.59	674			
TAPERED ACCELERATION LANE	N/A				X	1.24	1.32															99			
I-240 NB EXIT RAMP TO WB UNION AVENUE	30			X		1.423		14	36					86							1.36	380	45		
I-240 NB EXIT RAMP TO EB UNION AVENUE	30			X		1.069		14	57					81	17						0.25	742			
PARALLEL ACCELERATION LANE	N/A			X		1.22	1.27																229		
EB LAMAR AVE TO NB I-240	N/A			X		1.27				15				78	17						0.63	244			
WB LAMAR AVE TO NB I-240	N/A			X		1.433									16						0.15				
I-240 NB EXIT TO LAMAR AVE	29			X		1.627		14	55	24	1		165	48		1	1		12	16	24	0.29	406	31	
SB I-240 EXIT TO LAMAR AVE	29				X	1.072		42	52	25	3		195	81					15		0.61	623	50		
EB LAMAR ENTRANCE TO SB I-240	N/A				X	1.398				38				116							0.12	702			
PARALLEL ACCELERATION LANE	N/A			X		1.398	1.490															148			
WB LAMAR AVE ENTRANCE TO SB I-240	N/A				X	1.575				25				95	16						0.25	446			
PARALLEL ACCELERATION LANE	N/A				X	1.575	1.663															149			
DECELERATION LANE	28B				X	2.656	2.712			5													76		
SB I-240 EXIT TO COLLEGE ST	28B				X	2.712		14	39					137		1					0.28	458			
PARALLEL ACCELERATION LANE	N/A			X		2.673	2.767																158		
CUMMINGS ST EXIT TO NB I-240	N/A			X		2.767				18				91							0.17	303			
NB I-240 EXIT TO WB SOUTH PARKWAY	28B			X		2.854				14				37	15						0.25	235			
AUXILIARY LANE	28B				X	2.854	2.932			6														97	
EB SOUTH PARKWAY TO NB I-240	N/A			X		2.932				14				45	14						0.21	231			
WB SOUTH PARKWAY EXIT TO SB I-240	N/A				X	2.861				15				47							0.21	252			
AUXILIARY LANE	28A				X	2.861	2.936			6														93	
I-240 SB EXIT RAMP TO EB S PKWY E	28A				X	2.936				15				50							0.25	250			
NB I-240 EXIT RAMP TO CUMMINGS ST	28A			X		3.022				17				49							0.20	297			
DECELERATION LANE	28A			X		3.022	3.075			4													64		
SB I-240 ENTRANCE RAMP FROM COLLEGE ST	N/A				X	3.046				28				128							0.28	507			
PARALLEL ACCELERATION LANE	N/A			X		3.046	3.206																278		
SB I-240 EXIT TO NORRIS ROAD	26				X	4.585		14	45	24				126	36				36	24	0.28	323	37		
PARALLEL ACCELERATION LANE	N/A			X		4.606	4.650																80		
WB NORRIS ROAD EXIT TO NB I-240	N/A			X		4.650				16				62	42						0.10	279			
EB NORRIS ROAD EXIT TO SB I-240	N/A				X	4.849				18				63	36						0.15	303			
PARALLEL ACCELERATION LANE	N/A				X	4.849	4.936												36				155		
NB I-240 EXIT TO HERNANDO ROAD	26			X		5.179		14	43					177			1				0.20		531		
DECELERATION EXIT ONLY LANE TO HERNANDO ROAD	26					5.179	5.349			31							1						492	96	
SB I-240 EXIT TO I-55 NORTH ST. LOUIS EXIT 25B	25B				X	5.211				33				185							0.36	613	87		
I-240 NB ON RMAP ONLY	N/A			X		5.349				24				281							0.43	428		418	
EXIT ONLY LANE	6B				X	5.511	5.71			45				105									838	115	
EXIT ONLY LANE	24				X	5.71	6.00			30							1						1170	198	
TOTAL								154	961	97	8	3050	430	2	1	6	5	51	152	48	11.5	10504	1722	4319	909

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QUANTITIES

UTILITY

- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED ‘AROUND’ UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR’S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

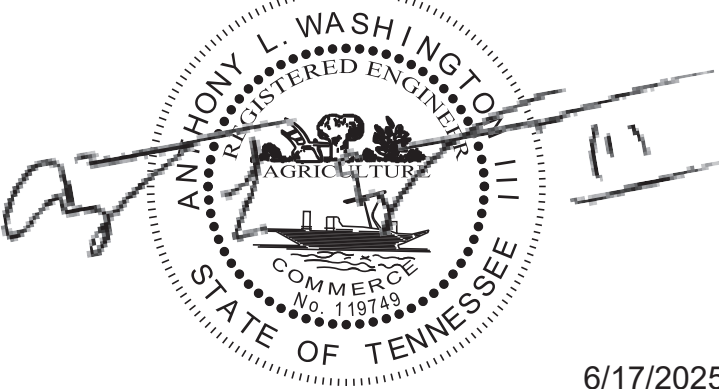
WATER, GAS, & ELECTRIC:
MLG & W
220 SOUTH MAIN ST.
MEMPHIS, TN. 38103
CONTACT: ANGEL BAILEY
OFFICE PHONE: 901 528 4186
CELL PHONE: 901 288 3419
Email: ADABNEY@MLGW.ORG

COMMUNICATIONS:
AT&T
315 E. COLLEGE STREET
JACKSON, TN. 38301
CONTACT: DANIEL R. POTTS
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COMMUNICATIONS:
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5450 WINCHESTER ROAD
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CONTACT: ANDREW SMITH
OFFICE PHONE: 901 623 7471
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STATE OF TENNESSEE
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UTILITY NOTES
AND
UTILITY OWNERS

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:

a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.

2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.

a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

a. THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.

b. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.

c. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.

d. THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.

FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

4. FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.

SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.

IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:

1. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 3/4 INCH AND NOT EXCEEDING 2 INCHES.

a. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

2. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:

a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

3. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:

a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b. ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:


1. WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

2. WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	79I240-S8-002	4

SEALED BY



6/17/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE
DROP-OFF NOTES
FOR
TRAFFIC CONTROL

6/17/2025 4:56:52 PM C:\PWORKING\EA\ST01\D343533\02D.DGN

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	79I240-S8-002	5

REV. 06-30-25: RENUMBERED SHEET.

NOTE: NOTIFICATION OF COMPLETION AND THE
X/Y COORDINATES OF ALL PULL BOXES IS REQUIRED
SEE SPECIAL TRAFFIC COUNTER SPECIFICATIONS



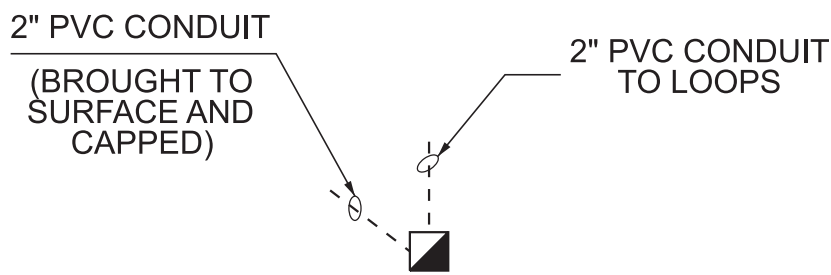
SOUTH BOUND

2 "

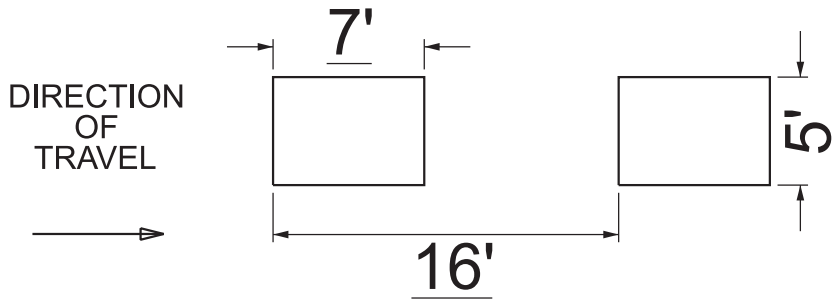
NB-3 □ □ NB-4
NB-1 □ □ NB-2

SHELBY COUNTY
I-240
RAMP #59 NB
North Bd Log Mile 5.67

NOT TO SCALE



PULL BOX DETAIL



LOOP DETAIL

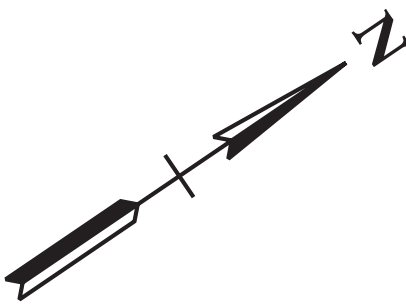
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC
DETECTION
LOOPS DETAIL

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	79I240-S8-002	5-1

REV. 06-30-25: RENUMBERED SHEET.

NOTE: NOTIFICATION OF COMPLETION AND THE
X/Y COORDINATES OF ALL PULL BOXES IS REQUIRED
SEE SPECIAL TRAFFIC COUNTER SPECIFICATIONS



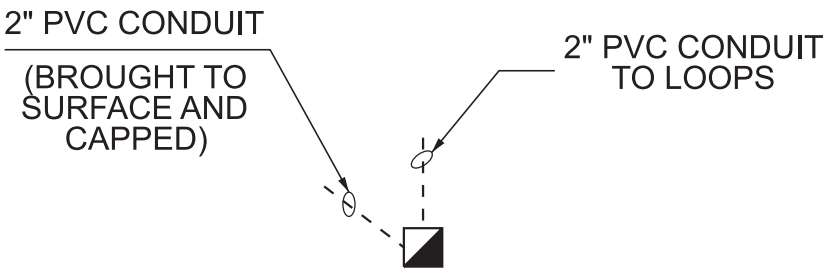
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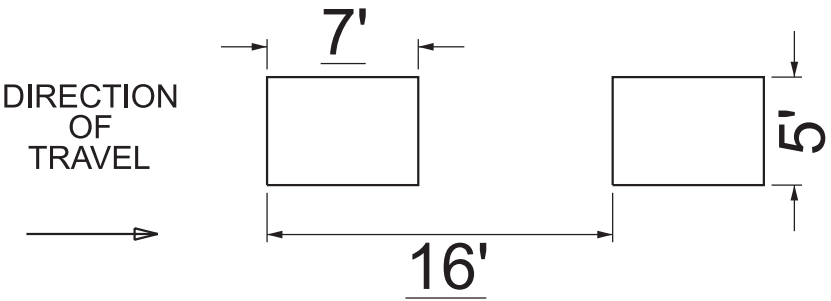
SB - 2 SB - 1

SHELBY COUNTY
RAMP 61
LM 5.35
FROM I-55 TO I-240

NOT TO SCALE



PULL BOX DETAIL



LOOP DETAIL

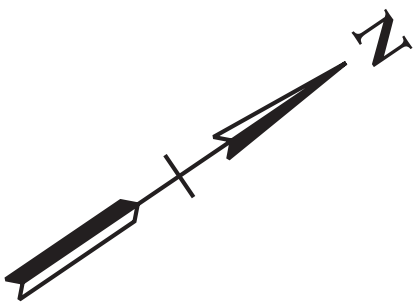
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC
DETECTION
LOOPS DETAIL

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	79I240-S8-002	5-2

REV. 06-30-25: RENUMBERED SHEET.

NOTE: NOTIFICATION OF COMPLETION AND THE
X/Y COORDINATES OF ALL PULL BOXES IS REQUIRED
SEE SPECIAL TRAFFIC COUNTER SPECIFICATIONS



SOUTH BOUND

2"



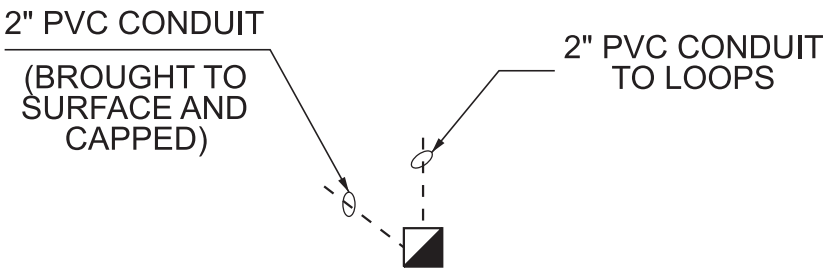
SB - 2



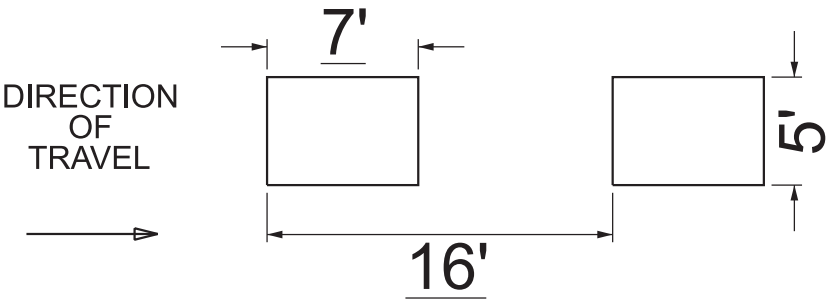
SB - 1

SHELBY COUNTY
RAMP 62
LM 5.28
FROM I-240 TO I-55

NOT TO SCALE



PULL BOX DETAIL



LOOP DETAIL

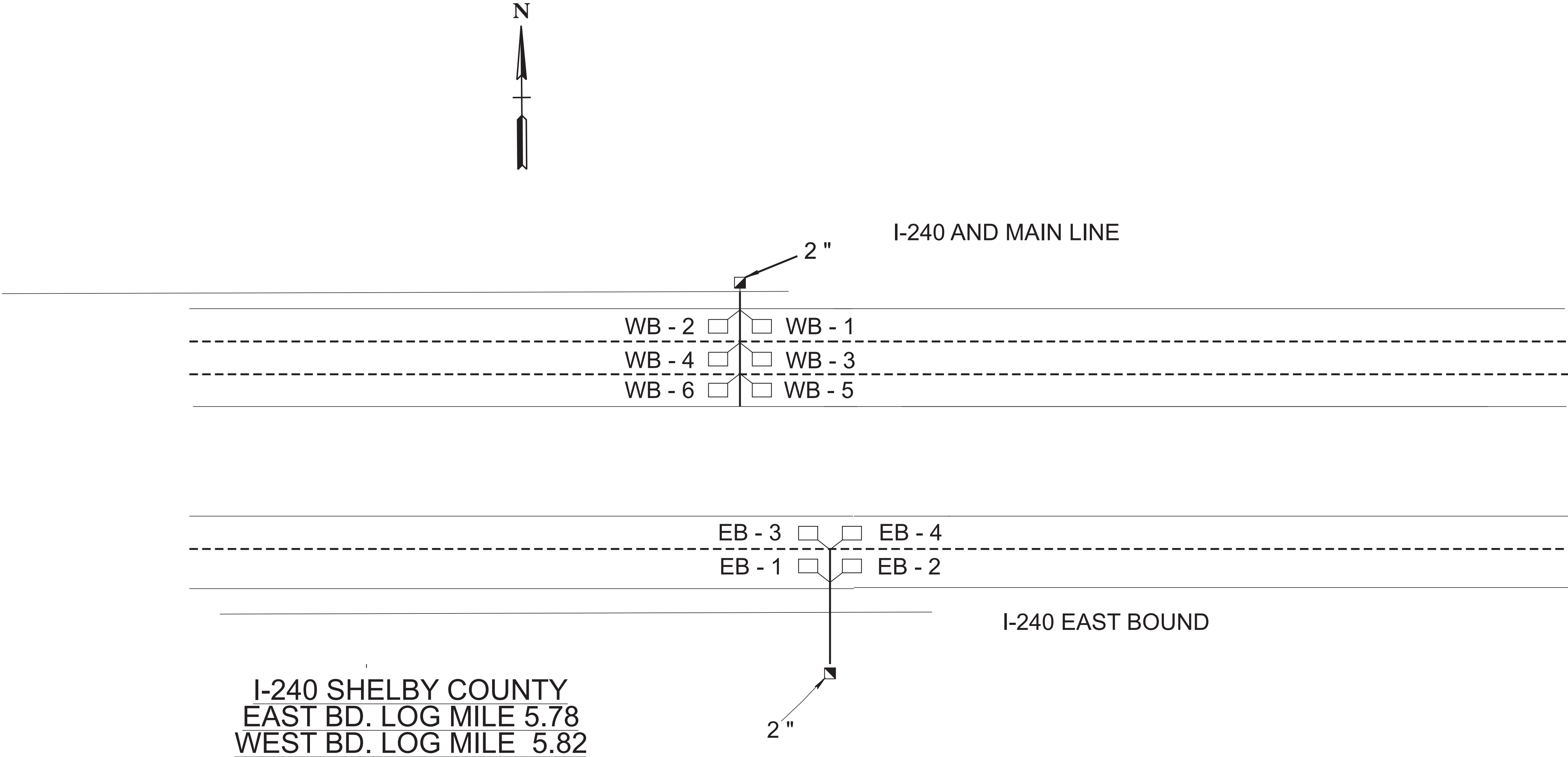
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC
DETECTION
LOOPS DETAIL

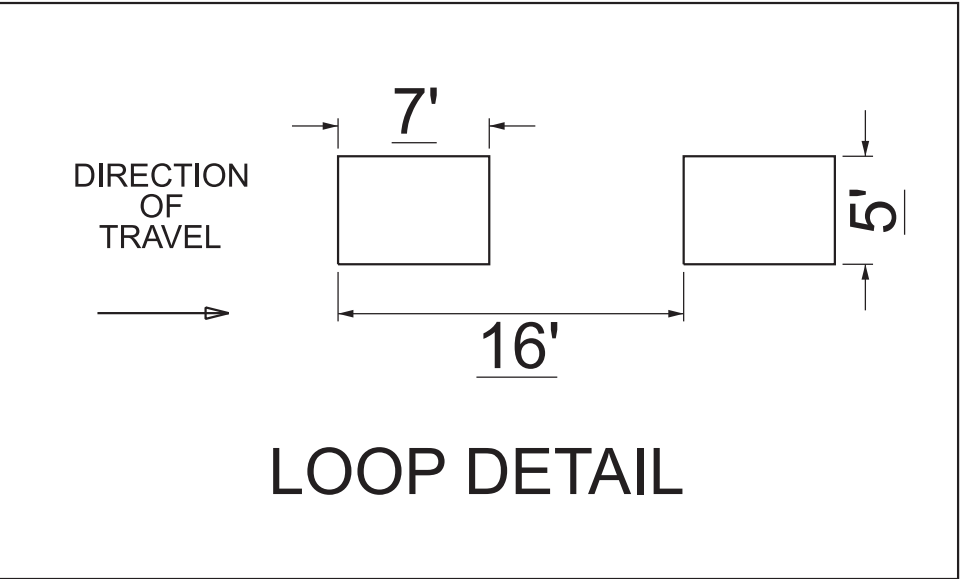
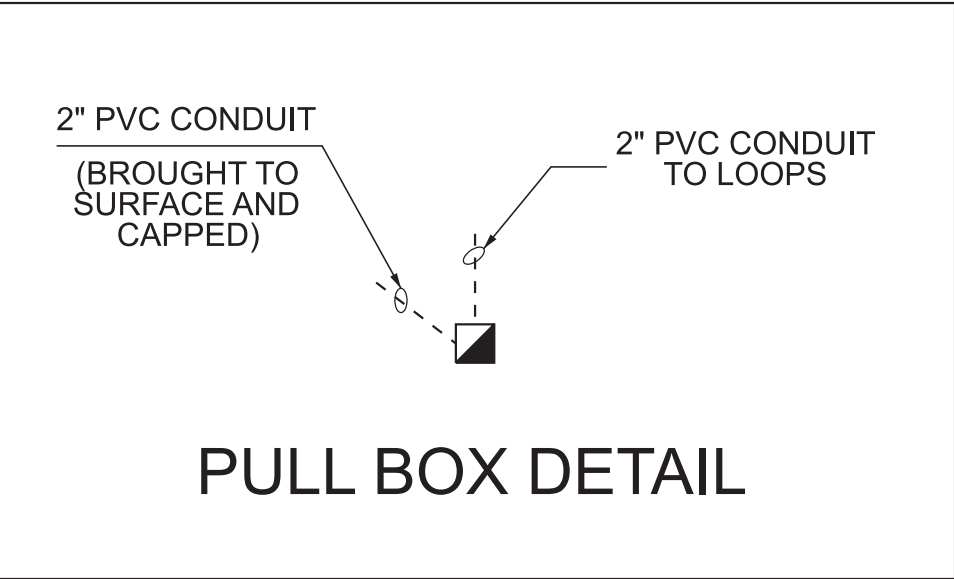
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	79I240-S8-002	5-3

REV. 06-30-25: RENUMBERED SHEET.

NOTE: NOTIFICATION OF COMPLETION AND THE
X/Y COORDINATES OF ALL PULL BOXES IS REQUIRED
SEE SPECIAL TRAFFIC COUNTER SPECIFICATIONS



NOT TO SCALE



STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION
TRAFFIC DETECTION LOOPS DETAIL



SIGNATURE SHEET _____ STRUCTURE-SIGN 6
BRIDGE PLANS _____ B1-B9

THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

Brian Egli

2025.07.01 11:38:21 -05'00'

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TENNESSEE DEPARTMENT OF TRANSPORTATION
505 DEADERICK STREET, SUITE 1200
NASHVILLE, TN 37243
BRIAN K. EGLI, P.E. NO. 107196

[illegible]

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE SHEET



SIGNATURE SHEET _____ STRUCTURE-SIGN 5
BRIDGE PLANS _____ B1-B3; B5; B7-B11

Brian Egli

2025.06.13 13:50:13 -05'00'

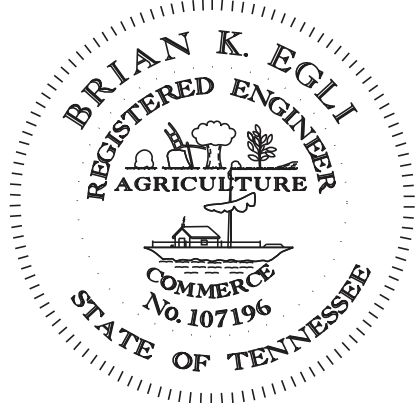
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TENNESSEE DEPARTMENT OF TRANSPORTATION
505 DEADERICK STREET, SUITE 1200
NASHVILLE, TN 37243
BRIAN K. EGLI, P.E. NO. 107196

[illegible]

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE SHEET



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:
Brian Egli
2025.05.22 08:47:27 -05'00'

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TENNESSEE DEPARTMENT OF TRANSPORTATION
505 DEADERICK STREET, SUITE 1200
NASHVILLE, TN 37243
BRIAN K. EGLI, P.E. NO. 107196

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.



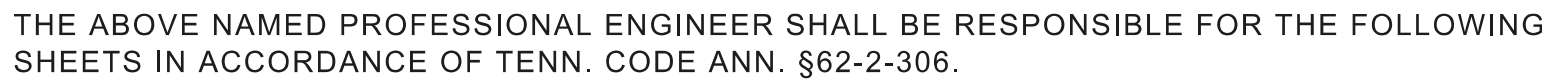
SHEET NAME	SHEET NO.
SIGNATURE SHEET _____	STRUCTURE-SIGN 4
BRIDGE PLANS _____	B1 THRU B16

YEAR	PROJECT NO.	SHEET NO.	
2025	79I240-M3-003	STRUCTURE-SIGN 4	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	5-20-25	ZH	ADDED SHEET

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET

\$\$\$\$SYTIME\$\$\$\$\$\$\$\$\$
\$\$\$\$SIGNSPEC\$\$\$\$\$\$\$\$\$

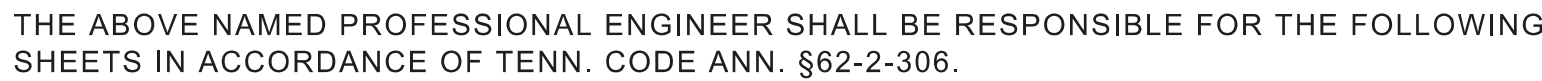


SHEET NO.

SIGNATURE SHEET _____ STRUCTURE-SIGN 3
BRIDGE PLANS _____ B1 AND B3

TENNESSEE DEPARTMENT OF TRANSPORTATION
505 DEADERICK STREET, SUITE 1200
NASHVILLE, TN 37243
BRIAN K. EGLI, P.E. NO. 107196

SIGNATURE SHEET



SIGNATURE SHEET _____ STRUCTURE-SIGN 2
BRIDGE PLANS _____ B1 AND B3

Brian Egli

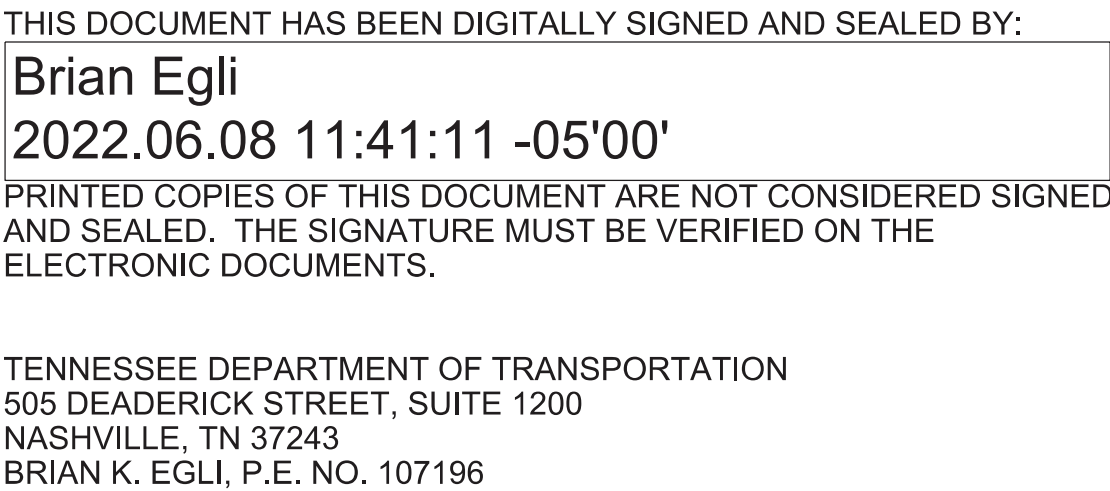
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TENNESSEE DEPARTMENT OF TRANSPORTATION
505 DEADERICK STREET, SUITE 1200
NASHVILLE, TN 37243
BRIAN K. EGLI, P.E. NO. 107196

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE SHEET



SHEET NAME	SHEET NO.
SIGNATURE SHEET _____	STRUCTURE-SIGN 1
BRIDGE PLANS _____	B1 THRU B15

YEAR	PROJECT NO.	SHEET NO.
2022	791240-M3-003	STRUCTURE-SIGN 1
STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION		
SIGNATURE SHEET		

PIN 129084.00

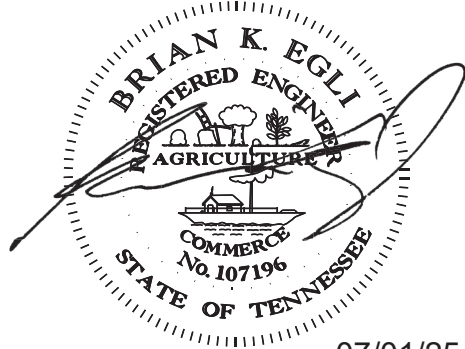


PROJECT NO.		YEAR	SHEET NO.
791240-M3-003		2025	B1
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	6-14-22	ZH	ADDED STRUCTURE-SIGN 2 & REVISION DATES
2	6-28-22	ZH	ADDED STRUCTURE-SIGN 3 & REVISION DATES
3	5-20-25	ZH	ADDED STRUCTURE-SIGN 4, REVISION DATES, & B7, REVISED TITLE & YEAR
4	6-13-25	ZH	ADDED STRUCTURE-SIGN 5 & REVISION DATES, REMOVED REFERENCE DRAWINGS
5	7-1-25	ZH	ADDED STRUCTURE-SIGN 6 & REVISION DATES, REMOVED B3; B5; B7-B11, REVISED PAGE NUMBERS

	INDEX OF DRAWINGS	DWG. NO.	LAST REV. DATE
5	SIGNATURE SHEET	STRUCTURE-SIGN 6	7-1-2025
2	SIGNATURE SHEET	STRUCTURE-SIGN 5	
1	SIGNATURE SHEET	STRUCTURE-SIGN 4	
3	SIGNATURE SHEET	STRUCTURE-SIGN 3	
4	SIGNATURE SHEET	STRUCTURE-SIGN 2	
	SIGNATURE SHEET	STRUCTURE-SIGN 1	
	INDEX OF DRAWINGS	B1	
	BRIDGE TABULATION AND ESTIMATED QUANTITIES	B2	7-1-2025
	APPROACH SLAB REPAIR DETAIL NOTES	B3	7-1-2025
	EXPANSION JOINT REPAIR DETAILS AT APPROACH ENDS NOTES	B4	7-1-2025
3	PLAN VIEW (79102400067)	B5	7-1-2025
	PHASE CONSTRUCTION	B6	7-1-2025
	PLAN VIEW (79100550038)	B7	7-1-2025
	PHASE CONSTRUCTION (SPANS 1 & 4)	B8	7-1-2025
	PHASE CONSTRUCTION (SPANS 2 & 3)	B9	7-1-2025



	INDEX OF REFERENCE DRAWINGS	DWG. NO.
4	LAYOUT OF BRIDGE	M-341-206
	SLAB PLAN	M-341-211
	LAYOUT OF BRIDGE	M-341-187
	TYPICAL SECTIONS	M-341-192



STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION
INDEX OF DRAWINGS
79-I240-1.80R&L
OVER
CSX RR & AGNES PL.
79-I240-5.34
OVER
I-55 WB RAMP
79-I240-5.48
OVER
I-55
BR. NOS. 79102400035
79102400036
79102400067
79100550038
SHELBY COUNTY
2025



DESIGNED BY _____ DATE _____
DRAWN BY Z.HAYNES DATE 6/22
SUPERVISED BY K. MARTINKO DATE 6/22
CHECKED BY _____ DATE _____

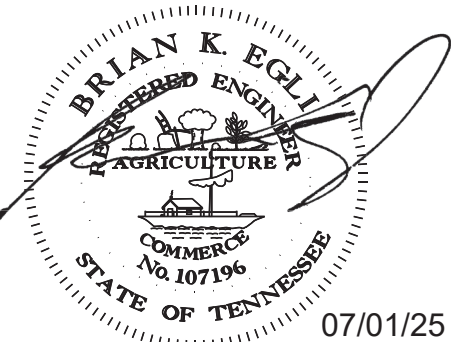
TABULATION OF BRIDGE RELATED WORK AND ESTIMATED QUANTITIES				
LOCATION OF BRIDGE AND BRIDGE NUMBER	REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS	TYPE OF WORK	604-10.44 EXPANSION JOINT REPAIR L.F.	604-10.53 CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PAVEMENT) S.Y.
79-I240-5.34 OVER I-55 WB RAMP (79102400067)	M-341-206 M-341-211	CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PAVMT.) EXPANSION JOINT REPAIR	102	8
79-I240-5.48 OVER I-55 (79100550038)	M-341-187 M-341-192	CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PAVMT.) EXPANSION JOINT REPAIR	131	5
TOTAL			233	13

PIN 129084.00

PROJECT NO.			YEAR	SHEET NO.
79I240-M3-003			2025	B2
REVISIONS				
NO.	DATE	BY	BRIEF DESCRIPTION	
1	5-20-25	ZH	ADDED 617-04.01 & REVISED TITLE & YEAR	
2	6-13-25	ZH	REMOVED 79102400035 & 79102400036 AND 604-10.56 & 617-04.01. ADDED TRAFFIC CONTROL QUANTITIES	
3	7-1-25	ZH	REVISED TRAFFIC CONTROL QUANTITIES	

TABULATED TRAFFIC CONTROL QUANTITIES			
ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	195
712-05.01	WARNING LIGHTS (TYPE A)	EACH	124
712-05.03	WARNING LIGHTS (TYPE C)	EACH	72
712-06	SIGNS (CONSTRUCTION)	S.F.	576
712-08.03	ARROW BOARD (TYPE C)	EACH	4

712-06 (CONSTRUCTION) TABULATION					
M.U.T.C.D. SIGN NO.	DESCRIPTION	QUANTITY	SIZE IN INCHES	UNIT	ITEM NO. 712-06 S.F.
W20-5R	X RIGHT LANE(S) CLOSED XX MI	4	48" X 48"	SF	64
W20-5R	X RIGHT LANE(S) CLOSED XX FT	4	48" X 48"	SF	64
W20-5L	X LEFT LANE(S) CLOSED XX MI	4	48" X 48"	SF	64
W20-5L	X LEFT LANE(S) CLOSED XX FT	4	48" X 48"	SF	64
W4-2R	RIGHT LANE CLOSED SYMBOL	10	48" X 48"	SF	160
W4-2L	LEFT LANE CLOSED SYMBOL	10	48" X 48"	SF	160
				TOTAL	576



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE TABULATION AND
ESTIMATED QUANTITIES
79-I240-1.80R&L
OVER
CSX RR & AGNES PL.
79-I240-5.34
OVER
I-55 WB RAMP
79-I240-5.48
OVER
I-55
BR. NOS. 79I02400035
79I02400036
79I02400067
79I00550038
SHELBY COUNTY
2025

[illegible]

B3

[illegible]

THE JOINT HEADER SYSTEM SHALL BE FROM OPL 9.002. THE JOINT SEAL SYSTEM CONSISTS OF THE ELASTOMERIC HEADER MATERIAL AND A TWO-PART COLD POUR SILICONE SEALANT. THE CONTRACTOR SHALL ALSO HAVE THE OPTION OF USING A PRECOMPRESSED FOAM WITH SILICONE TOPPED PRODUCT (SIMILAR TO AND INCLUDING BEJS) FROM OPL 7.001 WITH AN APPROPRIATELY SIZED SEAL FOR THE JOINT OPENING, AND AN ELASTOMERIC SEALANT FROM OPL 9.001. THE SYSTEM SHALL BE INSTALLED BY THE DIRECT SUPERVISION OF AN AUTHORIZED PERSON PROVIDED BY THE SYSTEM MANUFACTURER. THE TECHNICIAN MUST APPROVE ALL ASPECTS OF THE GEOMETRY AND PREPARATION OF THE JOINT LOCATIONS PRIOR TO ANY INSTALLATION OF THE JOINT SYSTEM MATERIALS. PRODUCTS FROM OPL 7.001 "ON A ROLL" AND "COMPRESSION ONLY" WILL NOT BE ALLOWED. THE TOP OF THE OPL 7.001 JOINT FILLER SHALL BE A MINIMUM OF 3/4 OF AN INCH BELOW THE ROADWAY SURFACE UNLESS THE MANUFACTURER REQUIRES A DEEPER DEPTH.

PRIOR TO THE INSTALLATION OF THE NEW JOINT, THE EXISTING JOINT OPENING SHALL BE CLEANED OF ALL DIRT, DEBRIS, AND PRIOR CONSTRUCTION MATERIAL, ETC., THE FULL DEPTH OF THE OPENING. THE SURFACES WHERE THE NEW MATERIAL BONDS TO STAY IN PLACE, SHALL BE CLEANED PER MANUFACTURERS RECOMMENDATION, TO REMOVE ANY SUBSTANCES THAT WOULD INHIBIT BONDING.

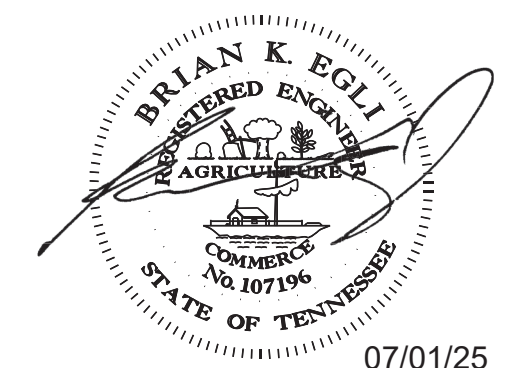
07/01/25

DEPARTMENT OF TRANSPORTATION
EXPANSION JOINT REPAIR DETAILS
AT APPROACH ENDS NOTES
79-I240-5.34
OVER
I-55 WB RAMP
79-I240-5.48
OVER
I-55
BR. NOS. 79I02400067
79I00550038
SHELBY COUNTY
2025

DESIGNED BY _____ DATE _____
DRAWN BY Z.HAYNES DATE 6/22
SUPERVISED BY K. MARTINKO DATE 6/22
CHECKED BY _____ DATE _____

[illegible]

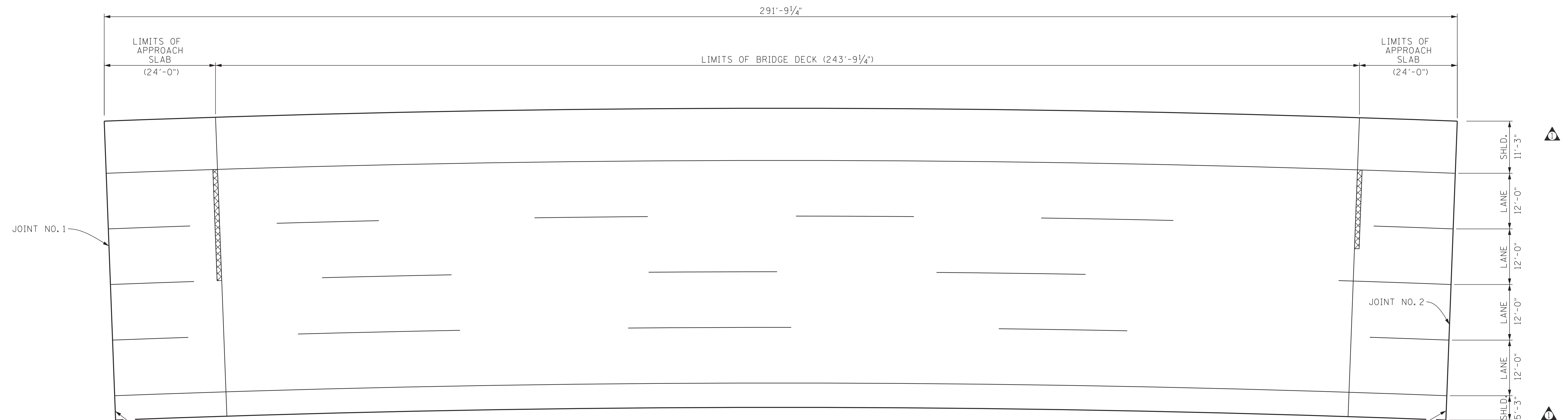
DESIGNED BY _____ DATE _____
 DRAWN BY Z.HAYNES DATE 6/22
 SUPERVISED BY K. MARTINKO DATE 6/22
 CHECKED BY _____ DATE _____



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PLAN VIEW
79-1240-5.34
OVER
I-55 WB RAMP
BR. NO. 79102400067
SHELBY COUNTY
2025



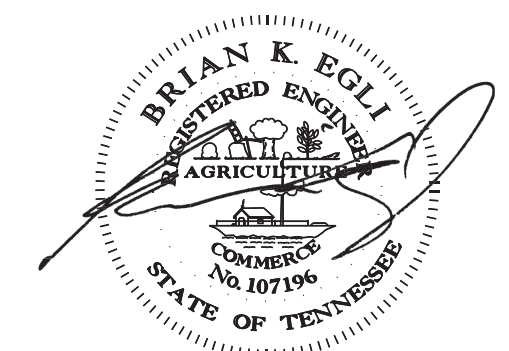
B6

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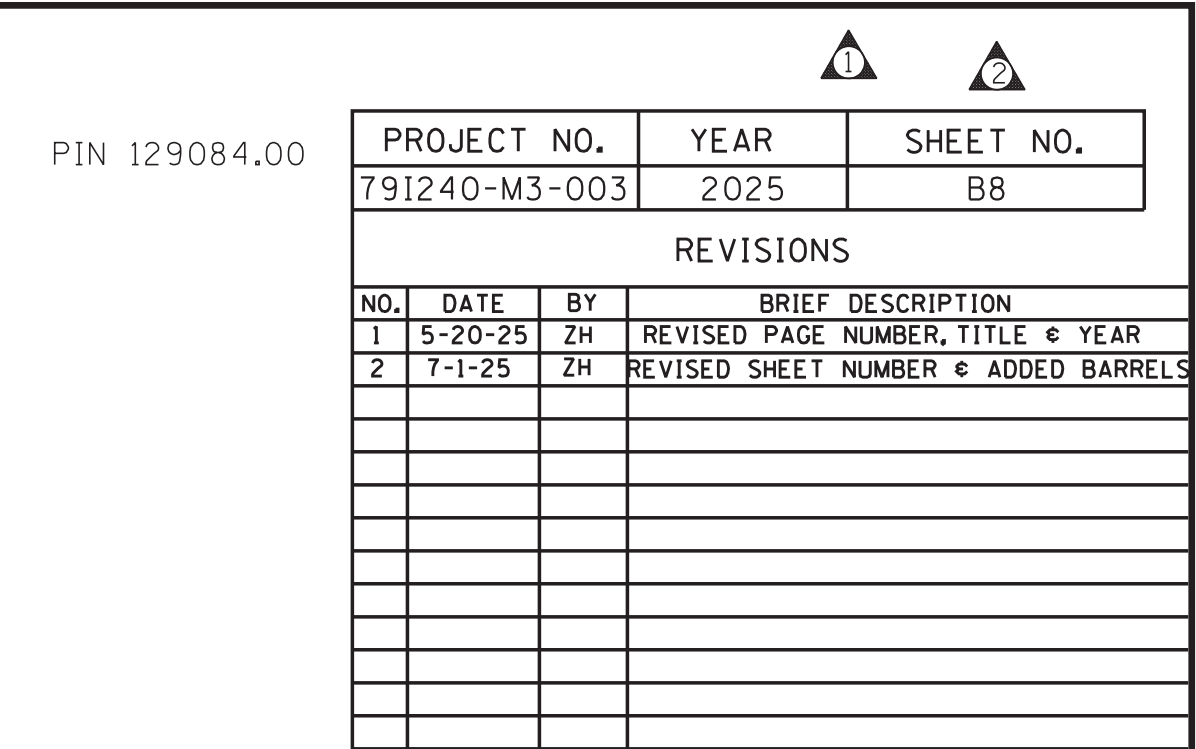
BRIDGE ID. NO. 79I00550038



DESIGNED BY _____ DATE _____
 DRAWN BY Z.HAYNES DATE 6/22
 SUPERVISED BY K. MARTINKO DATE 6/22
 CHECKED BY _____ DATE _____

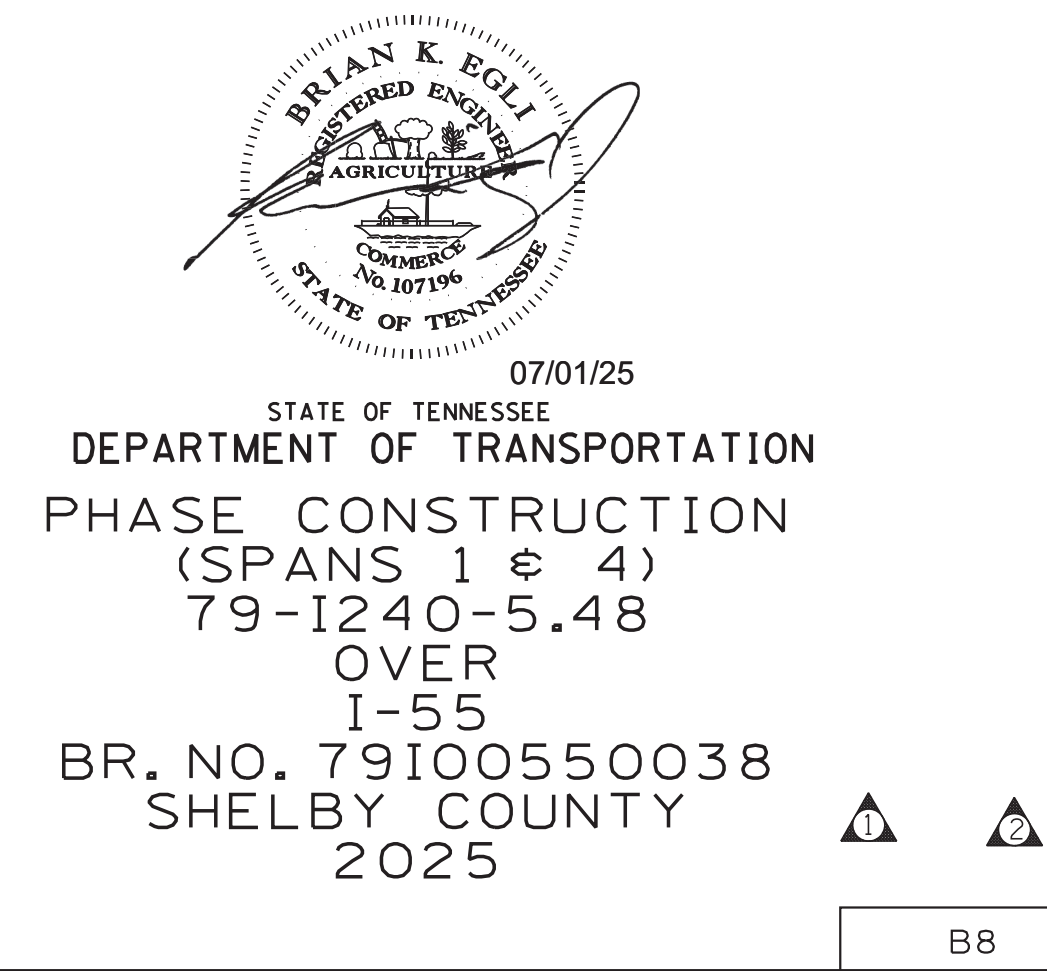


07/01/25
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PLAN VIEW
79-1240-5.48
OVER
I-55
BR. NO. 79100550038
SHELBY COUNTY
2025



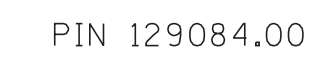
PHASE I CONSTRUCTION

(LOOKING AHEAD ON SURVEY)



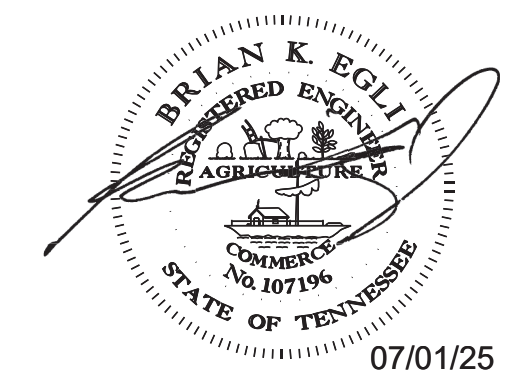
PHASE II CONSTRUCTION
(LOOKING AHEAD ON SURVEY)

DESIGNED BY _____ DATE _____
 DRAWN BY Z.HAYNES DATE 6/22
 SUPERVISED BY K. MARTINKO DATE 6/22
 CHECKED BY _____ DATE _____



PHASE I CONSTRUCTION

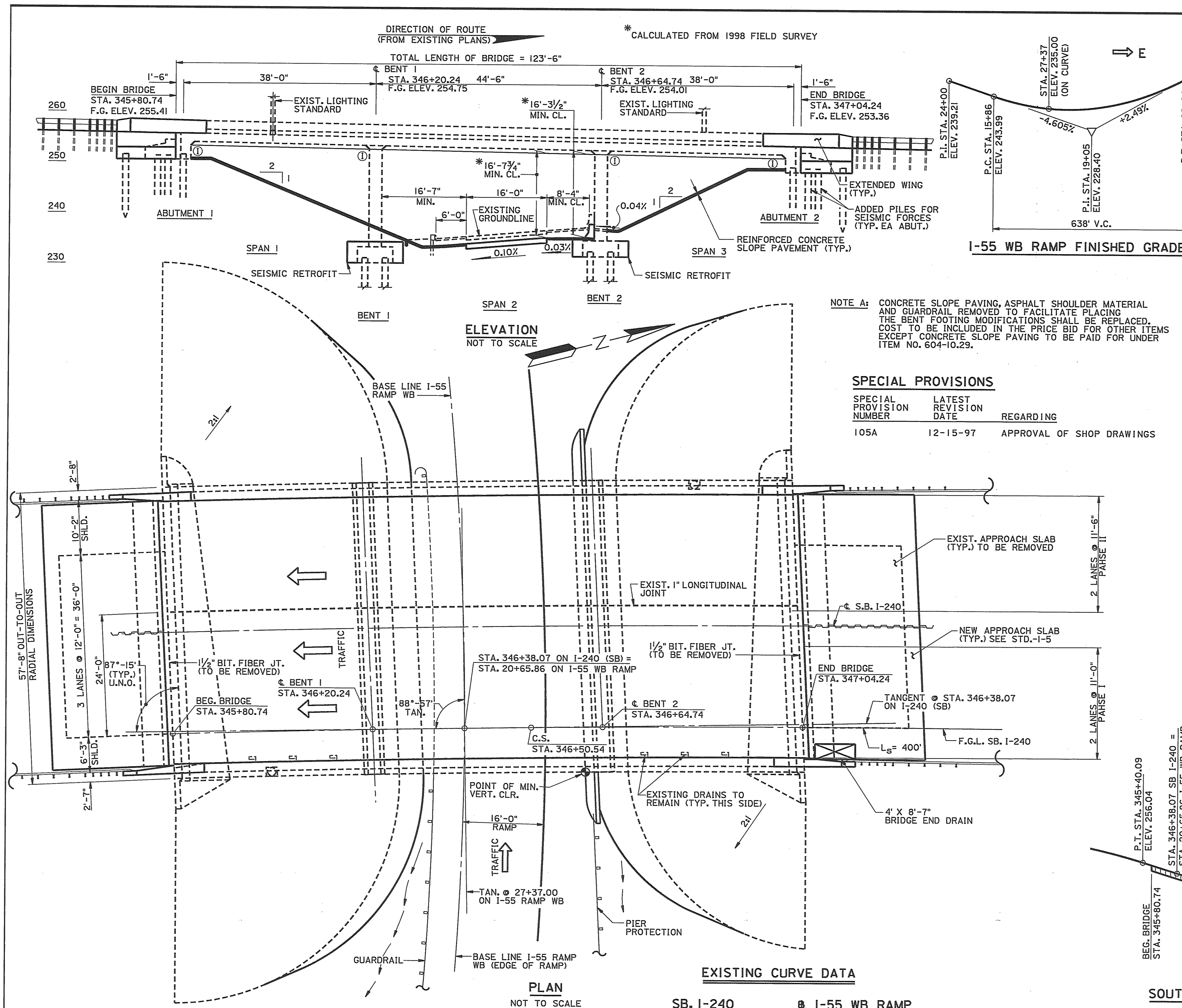
(LOOKING AHEAD ON SURVEY)



PHASE II CONSTRUCTION
(LOOKING AHEAD ON SURVEY)

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PHASE CONSTRUCTION
(SPANS 2 & 3)
79-I240-5.48
OVER
I-55
BR. NO. 79I00550038
SHELBY COUNTY
2025

DESIGNED BY _____ DATE _____
DRAWN BY Z.HAYNES DATE 6/22
SUPERVISED BY K. MARTINKO DATE 6/22
CHECKED BY _____ DATE _____



DE LEUW, CATHER
MEMPHIS, TENNESSEE

DESIGNED BY: J.R. PEGG DATE 5-98
DRAWN BY: D. RANDALL DATE 5-98
SUPERVISED BY: C.H. BRYANT DATE 5-98
CHECKED BY: C.H. BRYANT DATE 5-98

NOTE:
STATIONS, GRADES, SKEW ANGLES & OTHER LAYOUT
DATA TAKEN FROM 1958 CONSTRUCTION PLANS
UNLESS NOTED.

SB. I-240

R = 3819.72'

D = 1°-30'

V = 60 M.P.H.

e = 0.042%

I-55 WB RAMP

R = 674.068'

D = 8°-30'

V = 50 M.P.H.

e = 0.100%

SOUTHBOUND
I-240 FINISHED GRADE LINE
(ELEVATION BASED ON 1958 CONSTRUCTION PLANS,
SCARIFYING 1" OF EXISTING DECK AND ADDING 4 1/2"
CONCRETE OVERLAY.)

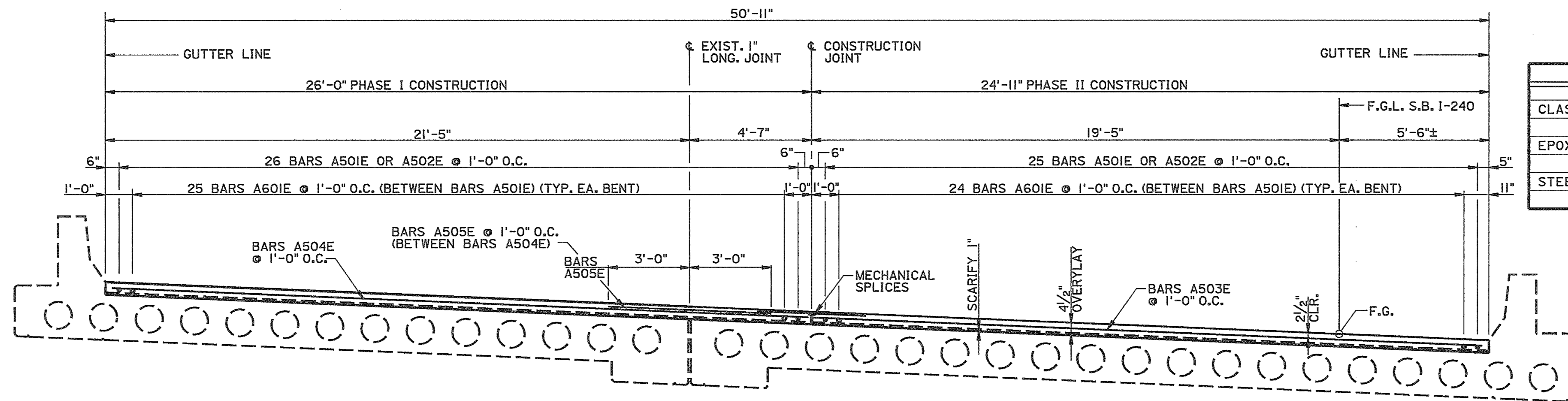


2018 ADT = 60800
52'-5" ROADWAY WITH STD-1-I PARAPETS
DESIGN SPEED = 60 M.P.H.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

LAYOUT OF BRIDGE
BRIDGE NO. 10

RETROFIT OF SB I-240 OVER I-55 WB RAMP
STA. 346+20.98 (L.M. 5.81)
BRIDGE ID. NO. 79102400067
SHELBY COUNTY
1998



TYPICAL SECTION

ESTIMATED QUANTITIES

DESCRIPTION	UNIT	QUANTITIES
CLASS "D" CONCRETE (BRIDGE DECK)	C.Y.	118
EPOXY COATED REINFORCING STEEL	LBS.	19710
STEEL BAR REINFORCEMENT (BRIDGES)	LBS.	770

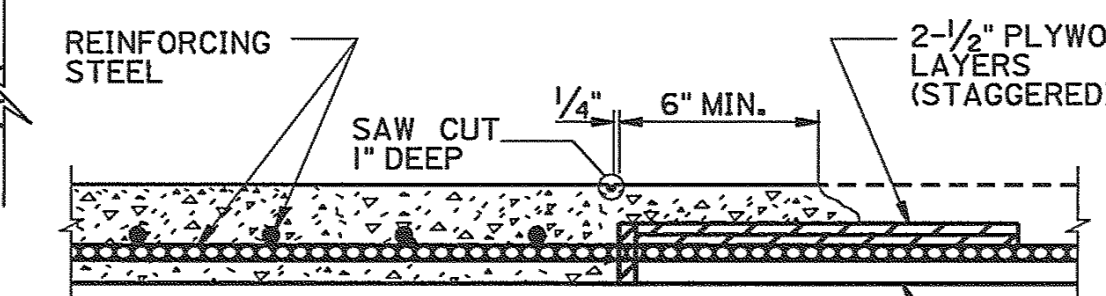
CONST. NO. 79004-3152-44

PROJECT NO.	YEAR	SHEET NO.
NH-1-55-110413	1998	

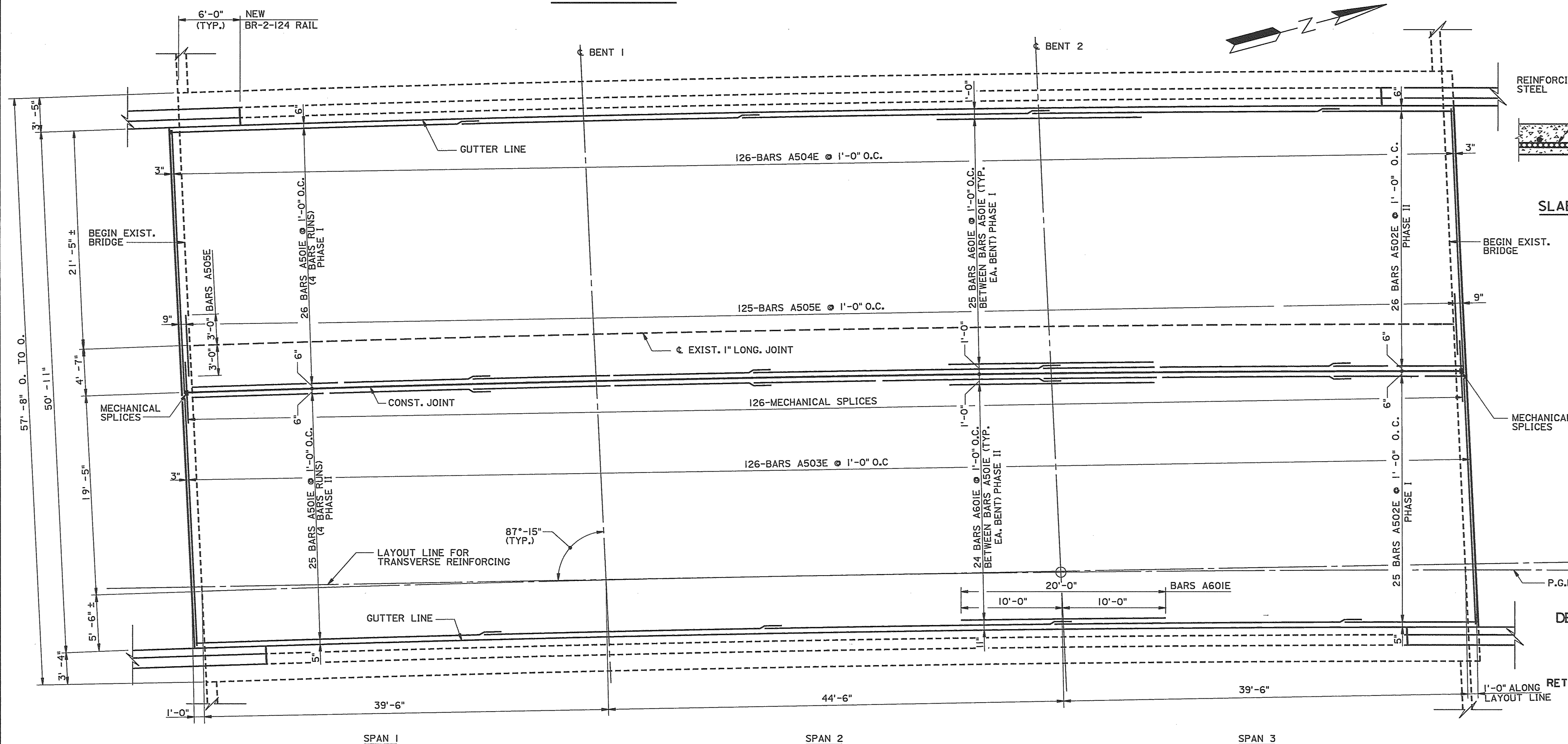
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

NOTES:

- THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS & SKEW ANGLES IN THE FIELD.
- MINIMUM LAP SPLICE LENGTH UNLESS NOTED OTHERWISE:
#5 BAR 2'-2"
- SPLICE TRANSVERSE BARS WITH MECHANICAL SPLICES. FOR DETAILS SEE DWG.
- SEE NOTE 11 ON DWG. M-341-208 FOR GROUTED BARS IN DRILLED HOLES.
- FOR REINFORCING SEE DWG. M-341-217.



TOP OF SCARIFIED DECK
SLAB CONSTRUCTION JOINT DETAIL



SLAB PLAN

DE LEUW, CATHAR
MEMPHIS, TENNESSEE

DESIGNED BY J.R. PEGG DATE 7-98
DRAWN BY T. MALONE DATE 7-98
SUPERVISED BY C.H. BRYANT DATE 7-98
CHECKED BY C.H. BRYANT DATE 7-98

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SLAB PLAN
BRIDGE NO. 10
RETROFIT OF SB I-240 OVER I-55 WB RAMP
STA. 346+20.98 (L.M. 5.81)
BRIDGE ID. NO. 79102400067
SHELBY COUNTY
1998



BEGIN BRIDGE STA. 336+87.03 F.G. ELEV. 257.75

TOTAL LENGTH OF BRIDGE 243'-9 1/4"±

END BRIDGE STA. 339+30.86 F.G. ELEV. 259.45

CONST. NO. 79004-3152-44

PROJECT NO.	YEAR	SHEET NO.
NH-1-55-1(104)3	1998	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	10-13-98	DR	ADDED REV. DATE
2	1/6/2000	MMF	ADDED SIGN NO.

SPECIAL PROVISIONS

SPECIAL PROVISION NUMBER	LATEST REVISION DATE	REGARDING
105A	12-15-97	APPROVAL OF SHOP DRAWINGS
602	3-18-96	SECTION 602-STEEL STRUCTURES
920	-----	ALTERNATE DETAILS - COLUMN SHELLS

LIST OF DRAWINGS

TITLE	DWG. NO.	LAST REV. DATE
LAYOUT OF BRIDGE	M-341-187	01/06/00
ESTIMATED QUANTITIES	M-341-188	5-20-99
GENERAL NOTES	M-341-189	
FOUNDATION DATA	M-341-190	
SEQUENCE OF CONSTRUCTION - SHEET 1 OF 2	M-341-191	
SEQUENCE OF CONSTRUCTION - SHEET 2 OF 2	M-341-191A	
TYPICAL SECTIONS	M-341-192	
SLAB PLAN	M-341-193	5-20-99
SUPERSTRUCTURE DETAILS - SHEET 1 OF 4	M-341-194	
SUPERSTRUCTURE DETAILS - SHEET 2 OF 4	M-341-195	
SUPERSTRUCTURE DETAILS - SHEET 3 OF 4	M-341-195A	
SUPERSTRUCTURE DETAILS - SHEET 4 OF 4	M-341-196	
FRAMING PLAN	M-341-197	
PRESTRESSED CONC. BOX BEAM (27" X 36") - SPANS 1 & 4	M-341-197A	
PRESTRESSED CONC. BOX BEAM (27" X 48") - SPANS 2 & 3	M-341-198	10-13-98
PRESTRESSED CONC. BOX BEAM (27" X 36") - SPANS 2 & 3	M-341-198A	10-13-98
ABUTMENT 1 DETAILS	M-341-199	
ABUTMENT 2 DETAILS	M-341-200	
ABUTMENT DETAILS	M-341-200A	
BENT DETAILS - SHEET 1 OF 3	M-341-201	
BENT DETAILS - SHEET 2 OF 3	M-341-202	
BENT DETAILS - SHEET 3 OF 3	M-341-202A	
FINAL FOUNDATION DATA	M-341-203	
BILL OF STEEL - SHEET 1 OF 2	M-341-204	
BILL OF STEEL - SHEET 2 OF 2	M-341-205	

LIST OF REFERENCE DRAWINGS

(1958 CONSTRUCTION PLANS)

TITLE	DWG. NO.	LAST REV. DATE
BRIDGE LAYOUT	G-6-13	3-02-82
GEOMETRY LAYOUT	G-6-14	6-05-58
ABUTMENTS A & E	G-6-15	6-05-58
PIERS B & D	G-6-16	6-05-58
PIER C	G-6-17	6-05-58
SUPERSTRUCTURE - SLAB - SPANS 1 & 2	G-6-18	6-05-58
SUPERSTRUCTURE - SLAB - SPANS 3 & 4	G-6-19	6-05-58
SUPERSTRUCTURE - SLAB DETAILS	G-6-20	6-05-58
PRESTRESSED BEAM - SPANS 1 & 2	G-6-21	6-05-58
PRESTRESSED BEAM - SPANS 3 & 4	G-6-22	6-05-58
BEARINGS & MISCELLANEOUS DETAILS	G-6-23	6-05-58
STANDARD 2 - RAIL HANDRAIL	G-6-24	6-05-58
LIGHTING & AUXILIARY DETAILS	G-6-25	6-05-58

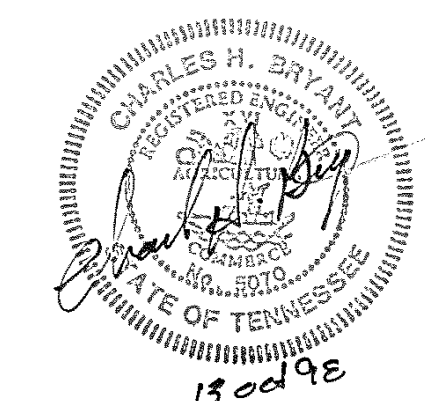
LIST OF STANDARD DRAWINGS

TITLE	DWG. NO.	LAST REV. DATE
DETAILS SHOWING PIER PROTECTION WITH NEW CONCRETE BARRIER WALL - 1988	SBR-2-127	1-04-96
DETAILS SHOWING PIER PROTECTION WITH NEW CONCRETE BARRIER WALL - 1988	SBR-2-128	1-04-96
BRIDGE RAILING CONCRETE PARAPET - 1990	STD-1-1	4-28-97
STEEL SLIDER PLATE ASSEMBLIES FOR CONCRETE PARAPET AND BRIDGE DECK DRAIN DETAILS - 1993	STD-1-2	9-11-95
REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS - 1995	STD-1-5	4-28-97
BRIDGE END DRAIN DETAILS 2' x 8' - 7" & 4' x 8' - 7" WITH PAVEMENT AT BRIDGE ENDS - 1993	STD-1-6	4-28-97
BRIDGE END DRAIN DETAILS 2' x 8' - 7" & 4' x 8' - 7" WITH PAVEMENT AT BRIDGE ENDS - 1993	STD-1-7	4-28-97
BRIDGE END DRAIN DETAILS 2' x 8' - 7" WITH PAVEMENT AT BRIDGE ENDS - 1993	STD-1-8	5-01-95
BRIDGE END DRAIN DETAILS 4' x 8' - 7" WITH PAVEMENT AT BRIDGE ENDS - 1993	STD-1-9	5-01-95
STANDARD PILE DETAILS - 1990	STD-5-1	10-25-93
STANDARD SEISMIC DETAILS - 1990	STD-6-1	5-01-95
STANDARD SEISMIC DETAILS - 1992	STD-6-2	11-07-94
LIGHT STANDARD SUPPORT DETAILS	STD-8-2	9-01-91
STANDARD REINF. BAR SUPPORT DETAILS FOR CONC. SLABS	STD-9-1	12-19-94
MISCELLANEOUS ABUTMENT & DRAINAGE DETAILS - 1971	STD-10-1	5-11-92

2018 ADT = 94000
68'-0" ROADWAY WITH STD-1-1 PARAPETS
DESIGN SPEED = 60 M.P.H.
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

LAYOUT OF BRIDGE
BRIDGE NO. 9

WIDENING OF S.B. I-55 OVER I-240
STA. 338+09.26 (L.M. 5.72 L)
BRIDGE ID. NO. 79100550038
SHELBY COUNTY
1998



EASTBOUND

I-240 FINISHED GRADE LINE
(AS SHOWN ON 1958 CONSTRUCTION PLANS)

WESTBOUND

I-55 FINISHED GRADE LINE
(ELEVATION BASED ON 1993 SURVEY, SCARIFYING 1" OF EXISTING DECK AND ADDING 4 1/2" CONCRETE OVERLAY.)

LEGEND:

- ⊕ FIXED BEARING (PROPOSED CONDITION)
- ⊞ DENOTES EXISTING DRAIN TO BE REMOVED
- DENOTES PROPOSED DECK DRAIN
- ⊞ BRIDGE END DRAIN
- ⊙ POINT OF MINIMUM VERTICAL CLEARANCE
- ~ SHEET PILES

ELEVATION

EXISTING SIGN AND SUPPORTS TO BE REMOVED AND SHALL BECOME THE PROPERTY OF THE CONTRACTOR. TO BE REPLACED WITH SIGN # 55. SEE DRAWING NUMBER M-352-113.

NOTE A:
CONCRETE SLOPE PAVING, ASPHALT SHOULDER MATERIAL AND GUARDRAIL REMOVED TO FACILITATE PLACING THE BENT FOOTING MODIFICATIONS SHALL BE REPLACED. COST TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS EXCEPT CONCRETE SLOPE PAVING TO BE PAID FOR UNDER ITEM NO. 604-10.29.

PLAN

* ALL ABUTMENTS AND BENTS ARE PARALLEL TO I-240

CURVE DATA - SB I-55

Dc = 1°-29'-43"
R = 3831.72'
e = 0.042X
DESIGN SPEED = 60 M.P.H.

BENCHMARK: GPS MONUMENT 79-055-13 APPROXIMATELY 100' NORTH OF I-240 AND APPROXIMATELY 300' WEST OF NORTHBOUND I-55. ELEV. 235.58.

DE LEUW, CATHER
MEMPHIS, TENNESSEE

DESIGNED BY J.R. PEGG DATE 6-98
DRAWN BY D. RANDALL DATE 6-98
SUPERVISED BY C.H. BRYANT DATE 6-98
CHECKED BY C.H. BRYANT DATE 6-98

